

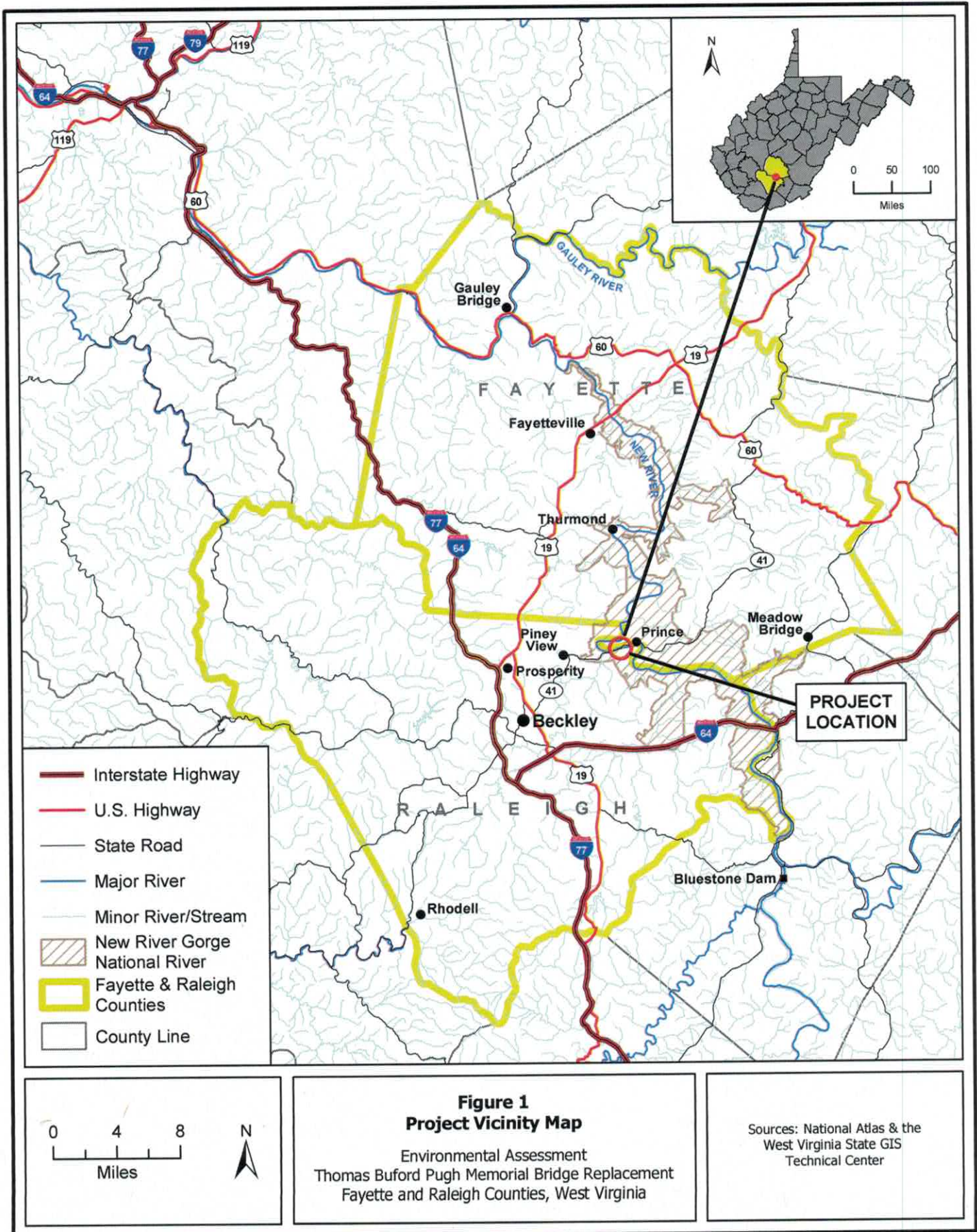
***Thomas Buford Pugh Bridge
Fayette and Raleigh Counties
State Project # S310-41-0.01
Federal Project # BR-0041(059)
FR#: 04-637-FA***



STATE LEVEL OF HISTORIC DOCUMENTATION

March 2015

***West Virginia
Department of Transportation
Division of Highways
Engineering Division
Environmental Section
(304) 558-2885***



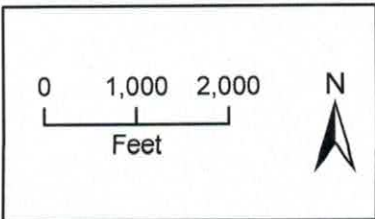
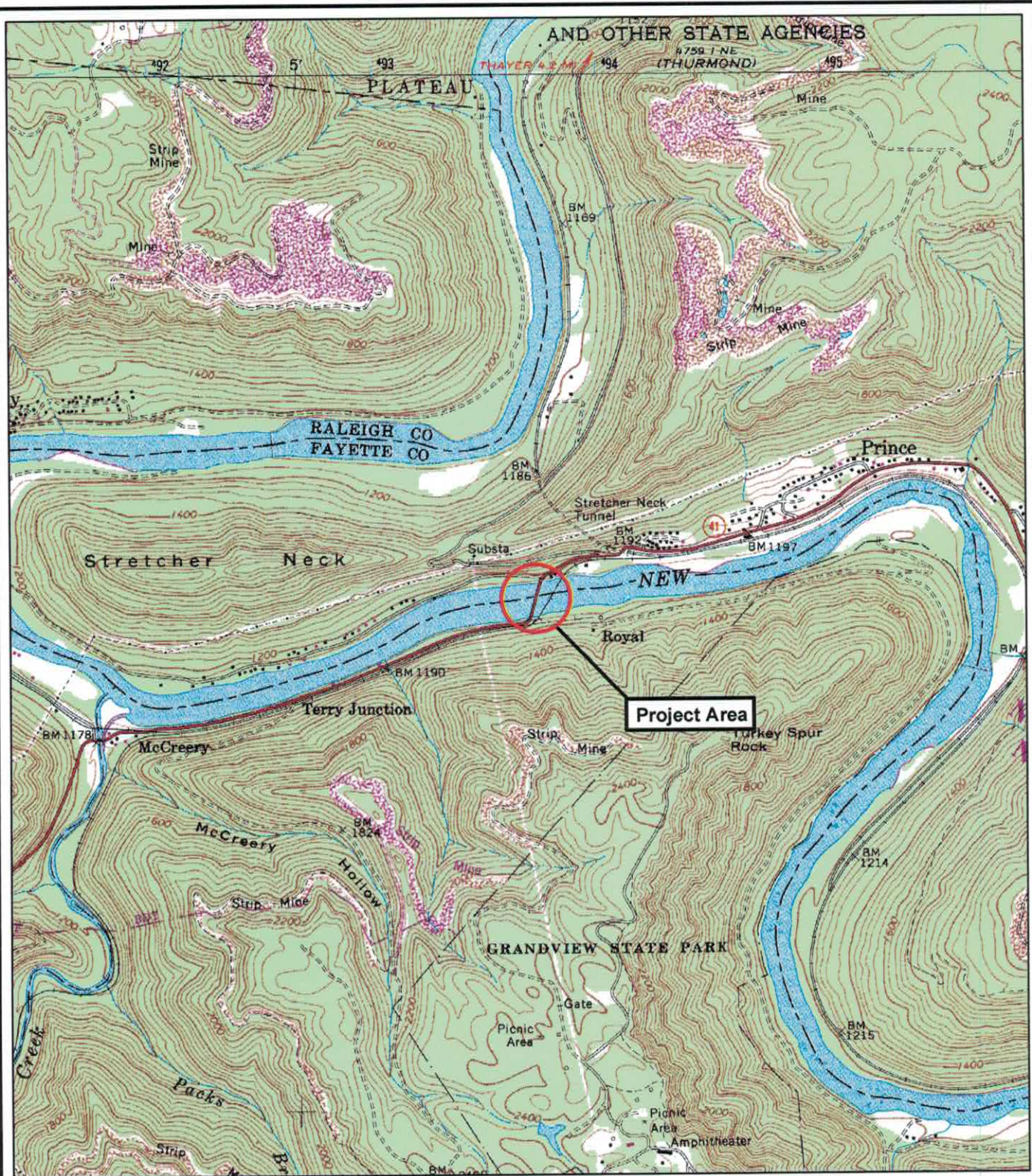


Figure 2
Project Area Map

Environmental Assessment
Thomas Buford Pugh Memorial Bridge Replacement
Fayette and Raleigh Counties, West Virginia

Sources: Prince & Thurmond, WV
USGS 7.5 Minute Quadrangles

STATE LEVEL HISTORIC DOCUMENTATION
THOMAS BUFORD PUGH BRIDGE
Also known as the PRINCE BRIDGE and PUGH BRIDGE

Location: West Virginia State Route 41, over New River
Near Prince
Fayette and Raleigh Counties
West Virginia

USGS Prince Quadrangle

Date of Construction: 1931

Builder: Commissioned by the Prince Bridge Company

Present Owner: West Virginia Department of Transportation
Division of Highways
1900 Kanawha Boulevard, Building 5, Room A-110
Charleston, WV 25305

Present Use: Vehicular Bridge

Significance: The Thomas Buford Pugh Bridge is significant for its engineering significance and as an example of a Parker Truss Bridge.

Project Information: The project has been undertaken due to the poor condition of the bridge. Any future deterioration of the bridge would result in its closure, the existing bridge warrants replacement. The documentation was undertaken in July 2012 in accordance with a Memorandum of Agreement among the West Virginia Department of Transportation and West Virginia State Historic Preservation Office. These measures are required prior to replacement of this National Register eligible structure.

Sondra L. Mullins, Structural Historian
West Virginia Division of Highways
Charleston, WV 25301
March 30, 2015

The Thomas Buford Pugh Bridge consists of a seven span Parker Truss Bridge. The spans are supported by reinforced concrete stub abutments, two reinforced concrete solid piers, two open column reinforced concrete piers, and two steel bents on reinforced concrete footings. The overall length is 734'3 1/2". The concrete filled steel grid deck has a roadway width of 20'3", an out to out width of 21'7", and a rail to rail width of 21'3". The structure has 8" x 9" reinforced concrete curbs, and flexes beam bridge rails, and no sidewalks. The structure has a stringer-floorbeam system with riveted connections and all stringers are framed into the floorbeams. The minimum vertical clearance is 9'0", and the vertical clearance is 9'0" for a 10' lane down the centerline of the deck. The average daily traffic (ADT) for this crossing is 950 (2012).



In September 1928, incorporation papers were filed and issued by the Secretary of State for the Prince Bridge Company. The company was organized for the purpose of building a highway bridge across the New River at Prince. The company's headquarters was in Fayetteville. The incorporators were Frank and James F. Prince of Prince, W.L. Lee of Fayetteville, J.D. Harrell of Charleston, and W. D. Lawton of Mount Hope (Charleston Daily Mail, September 2, 1928). In April 1929, the Assistant Secretary of War, Patrick Hurley approved the new bridge application (Charleston Gazette, April 26, 1929). In 1931 construction began on the bridge.

The bridge was completed in 1932 and served as a toll bridge. Originally, the tollbooth was connected to the bridge rail on the northwest side. The first toll master was Ira Clayton "Bud" Shuck. Other toll masters included Joe Jones and Charles Jones. The toll for the bridge was 25 cents for a motor vehicle, and 5 cents for a pedestrian. By August 1946, the bridge had paid for itself, collecting a total of \$246,350. When the tollbooth closed passage over the bridge became free. The tollhouse became the property of the toll keeper, Charles Jones, who rolled it over on logs across WV 41 to the opposite (northeast) corner of the bridge. The small two room structure became a playhouse for the owner's daughter. The tollhouse today is unrecognizable with the many additions over the years.

The bridge was known as the Prince Bridge until 1995. The bridge was renamed the Thomas Buford Pugh Bridge after Thomas Buford Pugh who was a local schoolteacher and World War II veteran who died in 1994. Mr. Pugh taught middle school science at both Beckley Junior High and Shady Spring Junior High Schools.

The Thomas Buford Pugh Bridge is eligible under Criterion C for its engineering significance as a Parker Truss Bridge.

BIBLIOGRAPHY

West Virginia Division of Highways, Bridge Files, Maintenance Division, Building 5, Capitol Complex, Charleston, West Virginia.

West Virginia Division of Highways, *Determination of Eligibility Report – Thomas Buford Pugh Memorial Bridge Replacement over the New River*, Engineering Division, Building 5, Capitol Complex, Charleston, West Virginia, November 2004.

West Virginia Division of Highways, *Draft Determination of Effect Report*, Engineering Division, Building 5, Capitol Complex, Charleston, West Virginia, September 2005.

STATE LEVEL HISTORIC DOCUMENTATION
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Thomas Buford Pugh Bridge
West Virginia State Route 41, over New River
Near Prince
Fayette and Raleigh County, West Virginia

Photographer: Randy Epperly

February 2014

PUGH -1	Elevation view looking upstream
PUGH -2	Elevation view looking downstream
PUGH -3	Bottom chord connections
PUGH -4	Sway bracing connections
PUGH -5	Bearing at pier
PUGH -6	Truss looking south
PUGH -7	End view of structure looking south
PUGH -8	Portal bracing
PUGH -9	Top chord connections
PUGH -10	End view of structure looking north
PUGH -11	Underneath the bridge looking north
PUGH -12	Underneath deck of bridge looking north
PUGH -13	Bent #1
PUGH -14	Elevation view looking north
PUGH -15	Abutment #1 portal vertical clearance on the southside of the bridge
PUGH -16	Sideview of the bridge showing railing

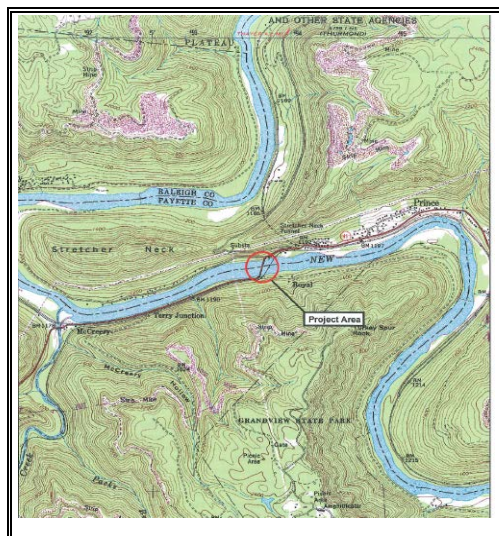
No original plans exist for this bridge. The bridge was built by a private company.

Internal Rating: _____



WEST VIRGINIA HISTORIC PROPERTY INVENTORY FORM

Street Address WV 41	Common/Historic Name/Both <input type="checkbox"/> <input type="checkbox"/> <input checked="" type="checkbox"/> Thomas Buford Pugh Bridge Prince Bridge Pugh Bridge	Field Survey #	Site # (SHPO Only)
Town or Community Near Prince	County Fayette and Raleigh Counties	Negative No.	NR Listed Date
Architect/Builder Commissioned by the Prince Bridge Company	Date of Construction 1931	Style (SHPO Only) Parker Truss Bridge	
Exterior Siding / Materials Steel truss bridge	Decking Material Concrete filled steel grid deck	Foundation	
Property Use or Function Transportation	UTM Zone 17 NAD 1981 Easting 493745E Northing 4189625N		
Survey Organization & Date WVDOH March 25, 2015	Quadrangle Name Prince		
	Part of What Survey / FR# State Project # S310-41-0.01 Federal Project # BR-0041(059)		



Name: Thomas Buford Pugh Bridge

Survey #:

Survey / FR#: FR #: 04-637-FA State Project Number: S310-41-0.01; Federal Project Number: BR-0041 (059)

Present Owners

WVDOT

Owners Mailing Address

1900 Kanawha Blvd. Room 110
Capitol Complex, Charleston, WV 25305

Describe Setting

Unknown -- <1 Acres

Archaeological Artifacts Present

The bridge carries West Virginia Route 41 over the New River near Prince, WV in Fayette and Raleigh Counties.

Description of Buildings or Site (Original and Present)

Stories

Front Bays

The Thomas Buford Pugh Bridge consists of a seven span Parker Truss Bridge. The spans are supported by reinforced concrete stub abutments, two reinforced concrete solid piers, two open column reinforced concrete piers, and two steel bents on reinforced concrete footings. The overall length is 734'3 1/2". The concrete filled steel grid deck has a roadway width of 20'3", an out to out width of 21'7", and a rail to rail width of 21'3". The structure has 8" x 9" reinforced concrete curbs, and flexes beam bridge rails, and no sidewalks. The structure has a stringer-floorbeam system with riveted connections and all stringers are framed into the floorbeams. The minimum vertical clearance is 9'0", and the vertical clearance is 9'0" for a 10' lane down the centerline of the deck. The average daily traffic (ADT) for this crossing is 950 (2012).

Alterations Yes No If yes, describe

The bridge has had several repairs over the years.

Additions Yes No If yes, describe

Describe All Outbuildings

N.A.

Statement of Significance:

The Thomas Buford Pugh Bridge is eligible under Criterion C for its engineering significance as a Parker Truss Bridge.

Bibliographical References

WVDOH Bridge Maintenance Files, 2015

Form Prepared By:

Date: March 25, 2015

Name/Organization: Sondra Mullins

Address: WV Division of Highways
1334 Smith Street
Charleston, WV 25301

Phone #: 304-558-9487

WEST VIRGINIA HISTORIC PROPERTY FORM CONTINUATION SHEET

Name: Thomas Buford Pugh Bridge
Survey Number:
Project / FR#: FR #: 04-637-FA State Project Number: S310-41-0.01; Federal Project Number: BR-0041 (059)



The Thomas Buford Pugh Bridge is eligible for the National Register of Historic Places under Criterion C for its engineering significance and as an example of a Parker Truss Bridge.



**MEMORANDUM OF AGREEMENT
BY AND AMONG
THE FEDERAL HIGHWAY ADMINISTRATION,
THE WEST VIRGINIA STATE HISTORIC PRESERVATION OFFICER, AND THE
WEST VIRGINIA DIVISION OF HIGHWAYS**

**REGARDING IMPLEMENTATION OF THE THOMAS BUFORD PUGH BRIDGE
REPLACEMENT PROJECT
STATE PROJECT #S310-41-0.01
FEDERAL PROJECT #BR-0041(059)E
FAYETTE COUNTY, WEST VIRGINIA
JULY 2012**

WHEREAS, the Federal Highway Administration (FHWA), in cooperation with the West Virginia Division of Highways (WVDOH), proposes to replace the Thomas Buford Pugh Bridge, which spans the New River in Fayette County, hereinafter referred to as the Project. The improvements involve the construction of a new bridge; and

WHEREAS, the FHWA has determined that the Project will have an adverse effect upon the Thomas Buford Pugh Bridge, a property eligible for the National Register of Historic Places (NRHP); and

WHEREAS, the FHWA has consulted with the West Virginia State Historic Preservation Officer (WVSHPO) pursuant to 36 CFR Part 800 Implementing Section 106 of the National Historic Preservation Act; (16 U.S.C., 470f); and

WHEREAS, the FHWA has determined that the Project will not effect archaeological properties; and

WHEREAS, in accordance with 36 CFR 800.6 (a) (1), the FHWA has notified the Advisory Council on Historic Preservation (ACHP) of its adverse effect determination providing the specified documentation, and the ACHP has chosen not to participate in the consultation pursuant to 36 CFR 800.6 (a) (1) (iii);

NOW, THEREFORE, the FHWA, the WVSHPO, and the WVDOH, agree that the undertaking will be implemented in accordance with the following stipulations in order to take into account the effects of the undertaking on historic properties.

STIPULATIONS

The FHWA shall ensure that the following stipulations are carried out:

Thomas Buford Pugh Bridge

- I. The Thomas Buford Pugh Bridge will be documented in its present historic setting. The documentation package will include 5"x7" black and white digital prints in

Thomas Buford Pugh Bridge Replacement

Memorandum of Agreement

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accordance with the National Register of Historic Places and National Historic Landmarks Survey Photo Policy Expansion of March 2005. Also, the documentation will be sent to the Fayette County Libraries, National Park Service, and the New River Gorge National River.

- II. A brief history of the structure will be included along with a fully completed West Virginia Historic Property Inventory form and copies of plan sheets and drawings of the bridge from WVDOH bridge files if available.
- III. The WVDOH will provide two historical interpretive markers designed by the National Park Service and placed along the sidewalk of the new bridge. The installation will be made part of the construction contract. The WWSHPO will have an opportunity to review and comment on the historic markers.
- IV. A brochure of the Thomas Buford Pugh Bridge will be developed and distributed to the National Park Service and the New River Gorge National River along with a CD version for future use. The WWSHPO will be given the opportunity to review all educational materials developed for this stipulation.
- V. Architectural treatments such as cut stone facing pattern wingwalls and abutments will be incorporated for the Thomas Buford Pugh Bridge, pending further coordination with the NPS and SHPO.

VI. Duration

This MOA will expire if its stipulations are not carried out within five (5) years from the date of its execution. At such time, and prior to work continuing on the undertaking, the FHWA shall either (a) execute a MOA pursuant to 36 CFR 800.6, or (b) request, take into account, and respond to the comments of the ACHP under 36 CFR 800.7. Prior to such time, FHWA may consult with other signatories to reconsider the terms of the MOA and amend it in accordance with Stipulation X. below. FHWA shall notify the signatories as to the course of action it will pursue.

VII. Post-Review Discoveries

If any unanticipated discoveries of historic properties or archaeological sites, including human burial sites and/or skeletal remains, are encountered during the implementation of this undertaking, work shall be suspended in the area of the discovery until the

WVDOH has developed and implemented an appropriate treatment plan in consultation with the WWSHPO pursuant to 800.13 (b).

Thomas Buford Pugh Bridge Replacement

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VIII. Monitoring and Reporting

Each year following the execution of this MOA until it expires or is terminated, FHWA shall provide all parties to this MOA a summary report detailing work carried out pursuant to its terms. Such report shall include any scheduling changes proposed, any problems encountered, and any disputes and objections received in FHWA's efforts to carry out the terms of this MOA.

VIV. Dispute Resolution

Should any signatory or concurring party to this MOA object at any time to any actions proposed or the manner in which the terms of this MOA are implemented, FHWA shall consult with such party to resolve the objection. If FHWA determines that such objection cannot be resolved, FHWA will:

- A. Forward all documentation relevant to the dispute, including the FHWA's proposed resolution, to the ACHP. The ACHP shall provide FHWA with its advice on the resolution of the objection within thirty (30) days of receiving adequate documentation. Prior to reaching a final decision on the dispute, FHWA shall prepare a written response that takes into account any timely advice or comments regarding the dispute from the ACHP, signatories and concurring parties, and provide them with a copy of this written response. FHWA will then proceed according to its final decision.
- B. If the ACHP does not provide its advice regarding the dispute within the thirty (30) day time period, FHWA may make a final decision on the dispute and proceed accordingly. Prior to reaching such a final decision, FHWA shall prepare a written response that takes into account any timely comments regarding the dispute from the signatories and concurring parties to the MOA, and provide them and the ACHP with a copy of such written response.
- C. FHWA's responsibility to carry out all other actions subject to the terms of this MOA that are not the subject of the dispute remain unchanged.

X. Amendments

This MOA may be amended when such an amendment is agreed to in writing by all signatories. The amendment will be effective on the date a copy signed by all of the signatories is filed with the ACHP.

Thomas Buford Pugh Bridge Replacement

Memorandum of Agreement

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XI. Termination

If any signatory to this MOA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other parties to attempt to develop an amendment per Stipulation VII, above. If within thirty (30) days (or another time period agreed to by all signatories) an amendment cannot be reached, any signatory may terminate the MOA upon written notification to the other signatories. Once the MOA is terminated, and prior to work continuing on the undertaking, FHWA must either (a) execute a MOA pursuant to 36 CFR 800.6, or (b) request, take into account, and respond to the comments of the ACHP under 36 CFR 800.7. FHWA shall notify the signatories as to the course of action it will pursue.

EXECUTION of the Memorandum of Agreement by the FHWA, WWSHPO, the WVDOH and the Council, and implementation of its terms evidence that the FHWA has afforded the Council an opportunity to comment on the Thomas Buford Pugh Bridge project and its effects on historic properties, and that the FHWA has taken into account the effects of the undertaking on the historic property.

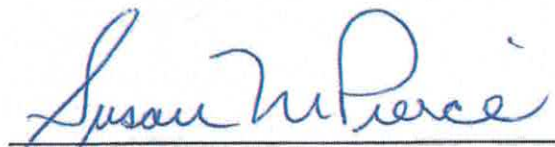
Thomas Buford Pugh Bridge Replacement
Memorandum of Agreement
Signatories Page

APPROVED:



Federal Highway Administration

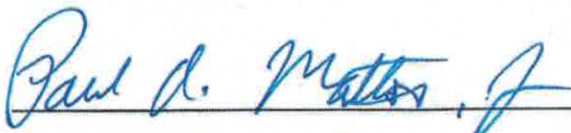
10/7/13
Date



West Virginia Deputy State Historic Preservation Officer

8/31/12
Date

CONCUR:



West Virginia Division of Highways

4/17/13
Date

Thomas Buford Pugh Bridge Replacement
Memorandum of Agreement
Signatories Page

CONSULTING PARTY:

Deborah A. Darden

National Park Service,
New River Gorge National River

7/26/2012

Date