

**STATE LEVEL HISTORIC DOCUMENTATION REPORT
NEW HOPE BRIDGE
MORGAN COUNTY, WEST VIRGINIA
STATE PROJECT No. S233-8/1-3.05
FEDERAL PROJECT No. STP-0081(100)D**



Prepared by:

Ahleah Boise, Historian

West Virginia Department of Transportation
Division of Highways
Technical Support Division
NEPA Compliance and Permitting Section
March 2025

STATE LEVEL HISTORIC DOCUMENTATION
NEW HOPE BRIDGE

Location: County Route 8/1 over Sleepy Creek, in Morgan County, West Virginia.
USGS Stotlers Crossroads Quadrangle

Date of Construction: 1916

Builder: The Concrete Steel Bridge Company, Clarksburg, West Virginia.

Present Owner: West Virginia Department of Transportation
Division of Highways
1900 Kanawha Boulevard, Building 5, Room A-110
Charleston, WV 25305

Present Use: Bridge

Significance: The New Hope Bridge is significant for its association with Frank D. McEnteer, a pioneer of reinforced concrete construction in West Virginia.

Project Information: The project has been undertaken due to the poor condition of the bridge. Future deterioration of the bridge could result in its closure, thus effectively closing Morgan County Route 8/1. This documentation was undertaken in April 2023 in accordance with a Memorandum of Agreement between the West Virginia Department of Transportation and West Virginia State Historic Preservation Office. These measures are required prior to replacement of this National Register eligible structure. The bridge is scheduled for replacement in 2025.

Ahleah Boise, Historian
West Virginia Division of Highways
Charleston, WV 25305
March 2025

The New Hope Bridge carries County Route 8/1 over Sleepy Creek in central Morgan County. Sleepy Creek is a tributary of the Potomac River. The bridge is located in a rural area west of Berkeley Springs known as New Hope. County Route 8/1 (Mountain Run Road) is a loop between New Hope and County Route 8 (Highland Ridge Road) at Duckwall to the south. The bridge is located 495 feet east of the intersection of County Route 8/1 with County Route 8. The bridge had an average daily traffic of 228 vehicles in 2021.

The New Hope Bridge is a two span, reinforced concrete closed spandrel arch bridge supported on full height reinforced concrete abutments and a solid reinforced concrete pier. Plaques embedded in the inside parapets at center read: "1916/MORGAN COUNTY COURT/J. H. PHILLIPS. PRES/J. W. SWAIM COMR./B.E. DAWSON "/M.S. HARMISON CLERK" and "THE CONCRETE STEEL/BRIDGE COMPANY/DESIGNER AND BUILDERS/CLARKSBURG W. VA." Each span measures 50 feet, and the overall span of the bridge is 108 feet, three inches. The solid concrete parapets have incised geometric panels at regular intervals. The bridge carries one lane of traffic and has a roadway width of 13.3 feet with an asphalt wearing surface. The bridge has steel strapping repairs applied to the upstream and downstream spandrel walls to stabilize movement. Steel braces are attached to the upstream footing to minimize separation of the abutment wingwall and spandrel wall.

Historic Context: Morgan County was established in 1820 from Hampshire County and Berkeley County. It is bordered to the east by Berkeley County, to the north by the Potomac River and Maryland beyond, and to the south and southwest by Frederick County, Virginia and Hampshire County, West Virginia. The county seat, Bath (typically called by its post office name of Berkeley Springs) is located in the northeast section, not far from the Potomac River. The county has one other incorporated community, Paw Paw, and numerous unincorporated communities.

Early native tribes in the region were the Huron, the Tuscarora, and the Mingo.¹ Prior to the Revolutionary War, the land that would become Morgan County was part of the Northern Neck Proprietary, a vast tract that was granted to several English noblemen by King Charles II in 1649. The land eventually came to be owned entirely by Thomas, the 6th Lord Fairfax, who divided it into smaller tracts. In 1747 Lord Fairfax moved to Virginia and managed his property from his home and office near present day Winchester.² The Fairfax lands were seized by the state of Virginia in 1779 and although Fairfax's heir, Denny Martin, attempted to regain control through a decades long legal battle, any land not already granted became the property of the Commonwealth of Virginia after a Supreme Court decision in 1816.³

With abundant springs fed by underground aquifers prevalent due to the karst topography, the area that would become Morgan County became a destination for health-seekers from the eastern seaboard seeking the curative effects of mineral water or simply escaping humid summers for the cooler mountain climate. The mineral springs at Bath developed into a popular resort by the 1770s and the town was incorporated in 1776.⁴ George Washington was part of a surveying party in the 1740s and returned to the springs at Bath throughout his life.

Berkeley Springs was incorporated in 1776. Always a destination for tourists visiting the mineral baths, the town grew into a local commercial hub after the arrival of the Baltimore & Ohio Railroad to Morgan County in the early 1840s. Industries included a tannery, a hosiery plant, a large canning factory, cold storage facility, and a silica quarry located north of town. In the late 20th century, the historic mineral springs are

¹ Dr. Robert Jay Dilger, History of Morgan County, Director, Institute for Public Affairs and Professor of Political Science, West Virginia University, October 2, 2002.

² Riddel, 48.

³ Ibid., 48.

⁴ Jeanne Mozier, "Morgan County" e-WV: The West Virginia Encyclopedia, 03 June 2013, web, 11 July 2022.

operated as a state resort park by the state of West Virginia, and Berkeley Springs has returned to its roots as an arts and heritage tourism destination.

Early transportation in Morgan County was via the Potomac River and local roads. The Winchester Grade Road was first incorporated as the Winchester and Berkeley Springs Turnpike in 1839.⁵ Other turnpikes through the county include the Morgan and Hampshire Turnpike, incorporated in 1851, and the Morgan and Frederick Turnpike, also incorporated in 1851. The turnpikes were supplemented by local and county roads already in use. The Baltimore & Ohio Railroad was built through Morgan County in the early 1840s, reaching Cumberland, Maryland by 1842. A spur line to Berkeley Springs was built in 1886.⁶

Outside of Berkeley Springs, much of Morgan County is agricultural. Sleepy Creek cuts a wide valley between Cacapon Mountain to the west and Sleepy Creek Mountain to the east as it meanders north from its headwaters near Bloomery, eventually meeting the Potomac River. Peach and apple orchards were prevalent in the 19th and mid-20th century. Rural Morgan County now consists of mostly low-density rural residential development, farmland in the Sleepy Creek valley, and vacation homes.

The New Hope Bridge is located approximately 480 feet southeast of New Hope Road on Mountain Run Road. Mountain Run Road is a very low volume rural road intersecting Highland Ridge Road to the east. The concrete double arch bridge was built in 1916 by the Concrete Steel Bridge Company of Clarksburg, West Virginia.

The New Hope Bridge is located in a rural area of Morgan County known as New Hope. New Hope is about five miles southeast of Berkeley Springs, the county seat. The area is a rural community with low density residential development and farmland.

Reinforced concrete single- and double-span arch bridges were built throughout the United States between the 1890s and the 1920s. This bridge type was frequently used in West Virginia due to its economic use of materials and low maintenance requirements.⁷

County Route 8/1 (Mountain Run Road) is a rural local road which provides access to residential properties and intersects County Route 8 (Spohrs Road) to the west and loops south to intersect County Route 8 again (in this section called Highland Ridge Road). At least three other double arch bridges found along Sleepy Creek were designed and built by the Concrete-Steel Bridge Company.

The New Hope Bridge is eligible for listing in the National Register of Historic Places under Criterion C. This double arch bridge has a continuous span design, representing an innovation in the engineering of concrete bridges. It was designed and constructed by the Concrete Steel Bridge Company of Clarksburg, WV, an engineering firm whose work is distinguishable on the state level. The Concrete Steel Bridge Company was a pioneer in the construction of reinforced concrete bridges and was awarded many projects throughout the state. The company specialized in shorter span bridges of 50 to 100 feet, many of which were single or double arches.

The Concrete Steel Bridge Company is regionally significant in West Virginia for its association with engineer Frank D. McEnteer. McEnteer was born in 1882 in Reynoldsville, Pennsylvania and attended Harvard University for an engineering degree. In 1911, McEnteer moved to Clarksburg, West Virginia

⁵ Acts of the General Assembly of the Commonwealth of Virginia, 1839, page 90.

⁶ Jacqueline Hovemale, "Berkeley Springs Train Depot" National Register of Historic Places Nomination Form, 2000.

⁷ Historic Context for Common Historic Bridge Types, 3-65 to 3-66.

to supervise construction of the Palace Furniture Company building, the first multi-story reinforced concrete building in the state. McEnteer was a consulting engineer for the York Bridge Company before founding the Concrete Steel Bridge Company in 1914. The firm went on to design and construct notable reinforced concrete bridges in West Virginia including the Fourth Street Bridge in Clarksburg and the Alderson Bridge in Greenbrier County. The Concrete Steel Bridge Company dissolved during the Great Depression due to financial difficulties. McEnteer spent the 1930s working for the West Virginia State Road Commission. During World War II, he served as the chief engineer of construction for the US armed forces in the Middle East. Following the war, McEnteer returned to Clarksburg and opened a consulting firm, which he ran until his death in 1957.

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INDEX TO PHOTOGRAPHS

New Hope Bridge
County Route 8/1 over Sleepy Creek
Morgan County, West Virginia

Photographer: Ahleah Boise, WVDOH

January 31, 2023

New Hope Bridge 1	South approach to bridge, view northwest
New Hope Bridge 2	West approach to bridge, view southeast
New Hope Bridge 3	Downstream elevation
New Hope Bridge 4	Upstream elevation
New Hope Bridge 5	View of west abutment and arch barrel
New Hope Bridge 6	Downstream view of pier and nose
New Hope Bridge 7	Metal plaque on north parapet
New Hope Bridge 8	Metal plaque on south parapet
New Hope Bridge 9	Detail of incised parapet
New Hope Bridge 10	Detail of parapet connection
New Hope Bridge 11	Upstream pier and nose
New Hope Bridge 12	Repair to west end of downstream spandrel
New Hope Bridge 13	North parapet
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Photo 1
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
South approach to bridge, view northwest



Photo 2
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
West approach to bridge, view southeast



Photo 3
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Downstream elevation



Photo 4
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Upstream elevation



Photo 5
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
View of west abutment and arch barrel



Photo 6
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Downstream view of pier and nose



Photo 7
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Metal plaque on north parapet



Photo 8
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Metal plaque on south parapet



Photo 9
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Detail of incised parapet



Photo 10
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Detail of parapet connection



Photo 11
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Upstream pier and nose



Photo 12
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
Repair to west end of downstream spandrel



Photo 13
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
North parapet



Photo 14
New Hope Bridge
Morgan County
County Route 8/1 spanning Sleepy Creek
South parapet



WEST VIRGINIA DEPARTMENT OF TRANSPORTATION
Division of Highways

1900 Kanawha Boulevard East • Building Five • Room 110
 Charleston, West Virginia 25305-0430 • (304) 558-3505

Gregory L. Bailey, P.E.
 Interim
 State Highway Engineer

Jimmy Wriston, P. E.
 Secretary of Transportation
 Commissioner of Highways

July 21, 2023

MEMORANDUM

TO: SEC/CC

FROM: DSN

THRU: DS, HD, CH

SUBJECT: State Project: S233-8/1-3.05
 Federal Project: STP-0081(100)D
 New Hope Bridge
 Morgan County

PREPARED BY: <i>S. Loggins</i>		
DATE: <i>07-21-23</i>		
ROUTE ORDER	OFFICE	OK
<i>1</i>	<i>DSN</i>	<i>SLM</i>
<i>2</i>	<i>DS</i>	<i>ti</i>
<i>3</i>	<i>HD</i>	<i>YH</i>
<i>4</i>	<i>CH</i>	<i>SLB</i>
<i>5</i>	<i>SEC</i>	<i>SDW</i>
<i>6</i>	<i>CC</i>	<i>SDW</i>
<i>7</i>	<i>HCC</i>	

Attached for your signature is Section 106 of the National Historic Preservation Act Memorandum of Agreement (MOA) for the subject project. Upon signing, please return to the Technical Support Division for further processing.

The Technical Support Division will transmit the environmental approval request for Categorical Exclusion, including the MOA, to the Federal Highway Administration for approval of both documents. This project will have an adverse effect to the New Hope Bridge, which is eligible for the inclusion in the National Register of Historic Places.

Should you have any questions, please contact Randy Epperly of our NEPA Compliance and Permitting Section at (304) 414-6439 or at Randy.T.Epperly@wv.gov.

TEL:el

Attachments

cc: DSN(RE)



WEST VIRGINIA DEPARTMENT OF TRANSPORTATION
Division of Highways





1900 Kanawha Boulevard East • Building Five • Room 110
Charleston, West Virginia 25305-0430 • (304) 558-3505

Gregory L. Bailey, P.E.
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July 21, 2023

MEMORANDUM

TO: SEC/CC 
FROM:  DSN
THRU:  DS, HD, CH 
SUBJECT: State Project: S233-8/1-3.05
Federal Project: STP-0081(100)D
New Hope Bridge
Morgan County

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Should you have any questions, please contact Randy Epperly of our NEPA Compliance and Permitting Section at (304) 414-6439 or at Randy.T.Epperly@wv.gov.

TEL:el

Attachments

cc: DSN(RE)



West Virginia Department of
**ARTS, CULTURE
AND HISTORY**

The Culture Center
1900 Kanawha Blvd., E.
Martinsburg, WV 25305-0300

Randall Reid-Smith, Curator
Phone: (304) 558-0720 • www.wvculture.org
Fax: (304) 558-7119 • TDD: (304) 558-3567

July 20, 2023

Mr. Travis Long
West Virginia Division of Highways
1900 Kanawha Boulevard East, Building 5, Room 110
Charleston, WV 25305
Via email: travis.e.long@wv.gov

RE: New Hope Bridge Replacement, Morgan County
State Project: S233-8/1-3.05; Federal Project: STP-0081(100)D
FR#: 23-0340-MN-1

Dear Mr. Long:

We have reviewed the *Memorandum of Agreement By and Among the Federal Highway Administration, and the West Virginia State Historic Preservation Officer and the West Virginia Division of Highways Regarding Implementation of the New Hope Bridge Replacement Project, Morgan County, West Virginia, June 2023* that was prepared for the above-referenced project. As required by Section 106 of the National Historic Preservation Act of 1966, as amended, and its implementing regulations, 36 CFR § 800: "Protection of Historic Properties," we submit our comments.

As detailed in our initial review letter, the West Virginia Division of Highways (WVDOH) is proposing to replace the New Hope Bridge, which carries County Route 8/1 (Mountain Run Road) over Sleepy Creek in a rural area of Morgan County. The existing bridge was constructed in 1916 by the Concrete Steel Bridge Company of Clarksburg, WV. We concurred that the New Hope Bridge (MN-0232) remained eligible for the National Register under Criterion C as a good example of a double arch bridge with a continuous span design constructed by the Concrete Steel Bridge Company..

Because the bridge is in such a deteriorated condition and is no longer structurally sound, the adverse effect cannot be avoided or minimized. Consequently, to address this adverse effect, WVDOH has developed a draft MOA to mitigate the adverse effect. We have reviewed the draft MOA and we are amenable to the proposed stipulations. These stipulations include:

- Documenting the bridge in its present historic setting including 5"x7" digital prints in accordance with the National Register of Historic Places and National Historic Landmarks Survey Photo Policy Expansion of January 2009. WVSHPO will be given an opportunity to review the documents before submission of final versions;
- A brief history of the structure will be included, along with a fully completed West Virginia Historic Property Inventory form with any available plan sheets and drawings from WVDOH bridge files;

July 20, 2023
Mr. Long
FR# 23-0340-MI-1
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- WVDOH will provide the Morgan County Historical Society, located within the Morgan County Public Library, with a copy of the New Hope Bridge State Level Documentation;
- Color brochures documenting the New Hope Bridge will be developed by WVDOH and distributed to the Morgan County Public Library. A CD or USB drive including a PDF digital copy of the brochure and bridge documentation package will also be provided. In addition, WVSHPO will be provided an opportunity to comment on all educational materials developed for this stipulation;
- The New Hope Bridge will be documented on the West Virginia historic bridge website;
- Bridge plaques will be donated to the Morgan County Historical Society.

Because we are in agreement with the proposed stipulations and as there are no additional parties who have been invited to comment on the draft MOA, we are enclosing a signed copy of the MOA with this letter. Once all parties have signed the MOA, please forward a copy to our office to be added to our files. We will provide further comments upon receipt of the documentation and information described within the stipulations of the MOA.

Consulting Parties/Public Comment:

We note that your office notified the Advisory Council on Historic Preservation (ACHP) of the adverse effect, and they have chosen not to participate. We understand that any further correspondence or comments regarding cultural resources will be sent to our office.

We appreciate the opportunity to be of service. *If you have questions regarding our comments or the Section 106 process, please contact Benjamin M. Riggle, Structural Historian, at (304) 558-0240.*

Sincerely,



Susan M. Pierce
Deputy State Historic Preservation Officer

SMP/BMR

CC: Sondra L. Mullins (sondra.l.mullins@wv.gov)
Randy T. Epperly (randy.t.epperly@wv.gov)

New Hope Bridge
Memorandum of Agreement
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**MEMORANDUM OF AGREEMENT
BY AND AMONG
THE FEDERAL HIGHWAY ADMINISTRATION,
THE WEST VIRGINIA STATE HISTORIC PRESERVATION OFFICER
AND THE WEST VIRGINIA DIVISION OF HIGHWAYS
REGARDING IMPLEMENTATION OF THE NEW HOPE
BRIDGE REPLACEMENT PROJECT
STATE PROJECT #S233-8/1-3.05
FEDERAL PROJECT #STP-0081(100)D
MORGAN COUNTY, WEST VIRGINIA
JUNE 2023**

WHEREAS, the Federal Highway Administration (FHWA), in cooperation with the West Virginia Division of Highways (WVDOH), proposes to remove the New Hope Bridge, spanning Sleepy Creek in a rural area of Morgan County, West Virginia, hereinafter referred to as the Project. The project will remove the existing bridge; and

WHEREAS, the FHWA has determined that the Project will have an adverse effect upon the New Hope Bridge, a property eligible for listing in the National Register of Historic Places (NRHP); and

WHEREAS, the FHWA has consulted with the West Virginia State Historic Preservation Officer (WVSHPO) pursuant to 36 CFR Part 800 Implementing Section 106 of the National Historic Preservation Act; (16 U.S.C., 470f); and

WHEREAS, the FHWA has determined that the Project will not affect archaeological properties; and

WHEREAS, The WVDOH has contacted the Preservation Alliance of West Virginia, Morgan County Historical Society, and the Morgan County Commission. None of the aforementioned groups have indicated their desire to be consulting parties; and

WHEREAS, in accordance with 36 CFR 800.6 (a) (1), the FHWA has notified the Advisory Council on Historic Preservation (ACHP) of its adverse effect determination providing the specified documentation, and the ACHP has chosen not to participate in the consultation pursuant to 36 CFR 800.6 (a) (1) (iii);

NOW, THEREFORE, the FHWA, the WVSHPO, and the WVDOH agree that the undertaking will be implemented in accordance with the following stipulations in order to take into account the effects of the undertaking on historic properties.

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Memorandum of Agreement
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STIPULATIONS

The FHWA shall ensure that the following stipulations are carried out:

I. New Hope Bridge Stipulations

- a. The New Hope Bridge will be documented in its present historic setting. The documentation package will include black and white digital photos in accordance with the National Register of Historic Places and National Historic Landmarks Survey Photo Policy Expansion of January 2009. The documentation package will include digital copies in the form of PDFs for reports and documents, and TIFF files for photographs. The WVSHPO will be given the opportunity to review the documents before submission of final versions.
- b. A brief history of the structure will be included along with fully completed West Virginia Historic Property Inventory forms and copies of any available plan sheets and drawings of the bridge from WVDOH bridge files.
- c. WVDOH staff will provide the Morgan County Historical Society located within the Morgan County Public Library with a printed copy of the New Hope Bridge State Level Historic Documentation for references and educational purposes.
- d. Color brochures of the New Hope Bridge will be developed by the WVDOH and distributed to the Morgan County Public Library. The WVSHPO will be given the opportunity to review all educational materials developed for this stipulation. A flash drive containing the brochure will also be provided to print brochures when the original total has been exhausted.
- e. Bridge plaques from the New Hope Bridge will be donated to the Morgan County Historical Society.
- f. The New Hope Bridge will be documented on the West Virginia historic bridge website, Highways Through History.

II. Duration

This MOA will expire if its stipulations are not carried out within five (5) years from the date of its execution. At such time, and prior to work continuing on the undertaking, the FHWA shall either (a) execute an MOA pursuant to 36 CFR 800.6, or (b) request, take into account, and respond to the comments of the ACHP under 36 CFR 800.7. Prior to such time, FHWA may consult with other signatories to reconsider the terms of the MOA and amend it in accordance with Stipulation VI below. FHWA shall notify the signatories as to the course of action it will pursue.

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Memorandum of Agreement
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III. Post-Review Discoveries

If any unanticipated effects to or discoveries of historic properties or archaeological sites, including human burial sites and/or skeletal remains, are encountered during the implementation of this undertaking, work shall be suspended in the area of the discovery until the WVDOH has developed and implemented an appropriate treatment plan in consultation with the WVSHPO pursuant to 36 CFR 800.13 (b).

IV. Monitoring and Reporting

Each year following the execution of this MOA until it expires or is terminated, FHWA shall provide all parties to this MOA a summary report detailing work carried out pursuant to its terms. Such report shall include any scheduling changes proposed, any problems encountered, and any disputes and objections received in FHWA's efforts to carry out the terms of this MOA.

V. Dispute Resolution

Should any signatory or concurring party to this MOA object at any time to any actions proposed or the manner in which the terms of this MOA are implemented, FHWA shall consult with such party to resolve the objection. If FHWA determines that such objection cannot be resolved, FHWA will:

- a. Forward all documentation relevant to the dispute, including the FHWA's proposed resolution, to the ACHP. The ACHP shall provide FHWA with its advice on the resolution of the objection within thirty (30) days of receiving adequate documentation. Prior to reaching a final decision on the dispute, FHWA shall prepare a written response that takes into account any timely advice or comments regarding the dispute from the ACHP, signatories and concurring parties, and provide them with a copy of this written response. FHWA will then proceed according to its final decision.
- b. If the ACHP does not provide its advice regarding the dispute within the thirty (30) day time period, FHWA may make a final decision on the dispute and proceed accordingly. Prior to reaching such a final decision, FHWA shall prepare a written response that takes into account any timely comments regarding the dispute from the signatories and concurring parties to the MOA, and provide them and the ACHP with a copy of such written response.
- c. FHWA's responsibility to carry out all other actions subject to the terms of this MOA that are not the subject of the dispute remain unchanged,

VI. Amendments

This MOA may be amended when such an amendment is agreed to in writing by all signatories. The amendment will be effective on the date a copy signed by all of the signatories is filed with the ACHP.

New Hope Bridge
Memorandum of Agreement
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VII. Termination

If any signatory to this MOA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other parties to attempt to develop an amendment per Stipulation XI, above. If within thirty (30) days (or another time period agreed to by all signatories) an amendment cannot be reached, any signatory may terminate the MOA upon written notification to the other signatories.

Once the MOA is terminated, and prior to work continuing on the undertaking, FHWA must either (a) execute a MOA pursuant to 36 CFR 800.6, or (b) request, take into account, and respond to the comments of the ACHP under 36 CFR 800.7. FHWA shall notify the signatories as to the course of action it will pursue.

EXECUTION of the Memorandum of Agreement by the FHWA, WVSHPO, the WVDOH and the Council, and implementation of its terms evidence that the FHWA has afforded the Council an opportunity to comment on the New Hope Bridge Project and its effects on historic properties, and that the FHWA has taken into account the effects of the undertaking on the historic properties.

New Hope Bridge
Memorandum of Agreement
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Signatories Page

JASON
WORKMAN

Digitally signed by
JASON WORKMAN
Date: 2024.11.22
14:52:10 -05'00'

Federal Highway Administration

Date



7/20/23

West Virginia Deputy State Historic Preservation Officer

Date

INVITED SIGNATORY:

DocuSigned by:

Jimmy D. Wriston

14291321DBBA40E...

West Virginia Division of Highways

7/24/2023

Date

DRAFT

LOCATION MAP

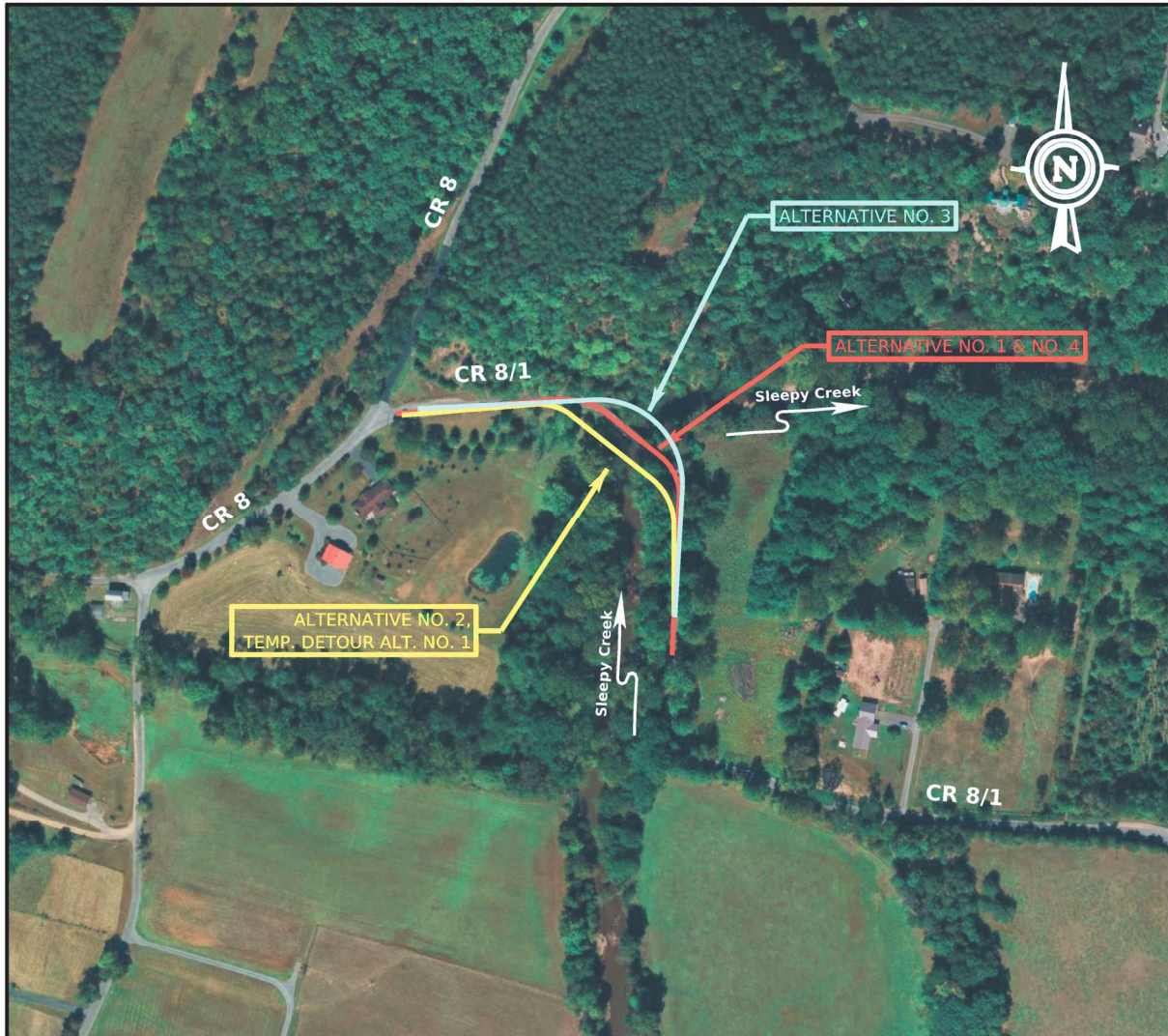
NEW HOPE BRIDGE

STATE PROJECT NO. S233-8/1-3.05 00

FEDERAL PROJECT NO. STP-0081(100)D

MORGAN COUNTY

LAT. 39°35'51.11" N LONG. 78°10'13.4" W



Alternative #1 replaces bridge at the existing location with a temporary detour upstream.
Alternative #2 replaces bridge at the upstream location with traffic maintained on the existing bridge.
Alternative #3 replaces bridge at the downstream location with traffic maintained on the existing bridge.
Alternative #4 replaces bridge at the existing location with traffic maintained on existing routes.

WEST VIRGINIA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
ENGINEERING DIVISION

WV SHPO Map-New Hope Bridge



January 23, 2023

1:2,257

0 0.01 0.03 0.06 mi



Architecture Survey - Area

Cemeteries - Area



Cemeteries - Point

National Register - Point

National Register - Area

Condition



Demolished



Active



Architecture Sites - Area



Demolished



Active

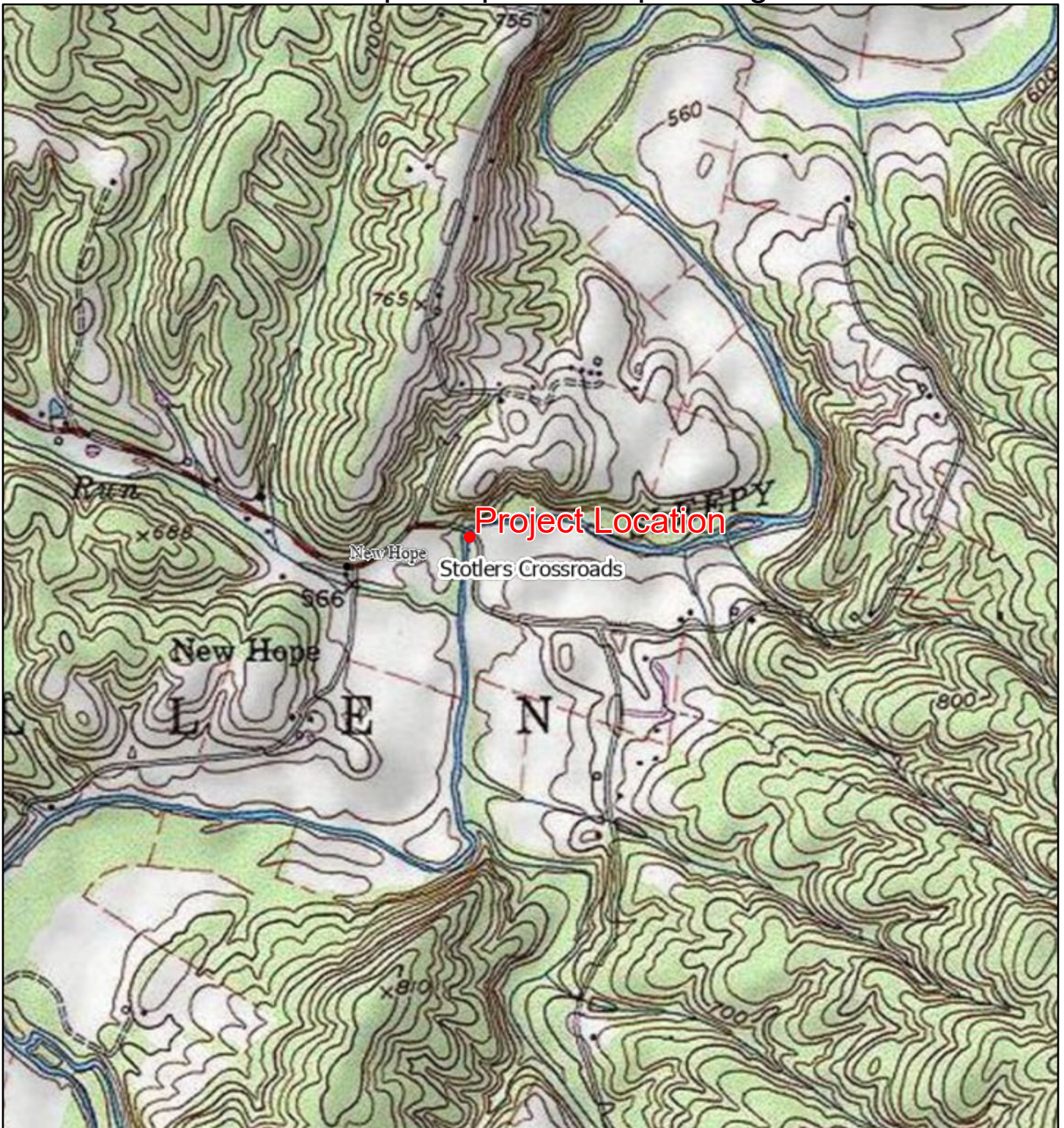


Architecture Sites - Point

Notes:

WVGISTC, WVSHPO

WV Topo Map-New Hope Bridge



January 23, 2023

1:18,056

0 0.13 0.25 0.5 mi

Notes:

West Virginia Historic Bridge Inventory Form

Bridge No. 33-008/01-003.05 BARS No. 33A015 Federal Bridge No. 00000000033A015 Bridge Design No. 7435.0

IDENTIFICATION INFORMATION

SHPO Survey No. MN-0232 Owner State Highway Agency
Local Name NEW HOPE BRIDGE Status Extant - in service
Other Local Name

LOCATIONAL AND SETTING INFORMATION

District 05 County Morgan Latitude 39354800 Longitude 078101200
Location 0.08 MI S OF CR 08 SLS UTM-Northing
Facility Carried By Structure CR 8/1 SLS UTM-Easting
UTM Zone
Features Intersected SLEEPY CREEK Surrounding Land Use Residential
Type of Development Rural - (undeveloped area outside communities)

STRUCTURAL INFORMATION

Main Span Type Concrete Arch - Deck (continuous) Structure Length (ft) 108
Main Span Type Code 211 Length of Maximum Span (ft) 50
Number of Spans in Main Unit 002 Average Daily Traffic 000250 Year 2002
Number of Approach Spans 0000 Sufficiency Rating 0197 Skew 00
(Note: Data current as of April 2006 database)

BRIDGE DESCRIPTIVE INFORMATION

Year Built 1916 Arrangement
Year Reconstructed Connection Type
Truss Bridge Type Truss Details
Alteration(s) Date of Alterations (Year)

Architectural Treatment(s)

Decorative pier nose

Bridge Plate Text

(2) plaques. "1916, MORGAN COUNTY COURT, J.H. PHILLIPS PRES, J.W. SWAITH COMR, B. E. DAWSON, M.S. HARMISON CLERK" and "THE CONCRETE STEEL, BRIDGE COMPANY, DESIGNERS AND BUILDERS, CLARKSBURG, W. VA."

BRIDGE HISTORY

Engineer or Designer Builder or Fabricator Concrete Steel Bridge Company
Bridge Plan Location Unknown
Additional Details: Closed spandrel arch. Concrete parapet with incised panels. Decorative pier nose is deteriorated. Bracing on west elevation channels. Exposed rebar on parapet. Deck patch on north side. Pier on west side eroded.

NATIONAL REGISTER EVALUATION INFORMATION

National Register Determination	Eligible	Reason Not Evaluated
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National Register Determination Date	2013
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This bridge is not eligible for the National Register under Criterion A as it does not have a significant association with an important historic transportation system, program, event, trend, or policy identified through contextual research and survey activities.

This bridge exhibits continuous span design, demonstrating important engineering design or technology as a variation within its class.

This bridge was designed or constructed by a known regional or West Virginia-based engineer, architect, or firm whose work is recognized as distinguishable within the state of West Virginia.

This bridge displays one or more architectural treatments.

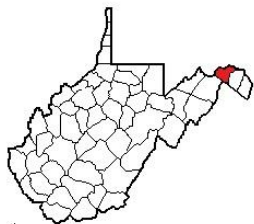
Although this bridge has undergone alterations, it still retains the historic integrity necessary to convey its engineering significance and, therefore, is eligible for the National Register under Criterion C.



West Virginia Historic Bridge Inventory Form


Form Prepared By Mead & Hunt and KCI

Form Preparation Date 2013



Internal Rating: _____

WEST VIRGINIA HISTORIC PROPERTY INVENTORY FORM

Street Address County Route 8/1 over Sleepy Creek	Common/Historic Name/Both <div style="text-align: center;"> <input checked="" type="radio"/> <input type="radio"/> <input type="radio"/> </div> New Hope Bridge	Field Survey #	Site # (SHPO Only) MN-0232	
Town or Community New Hope	County Morgan	Negative No.	NR Listed Date N/A	
Architect/Builder Concrete Steel Bridge Co.	Date of Construction 1916	Style Reinforced concrete double arch		
Exterior Siding/Materials Concrete	Roofing Material Concrete	Foundation Concrete		
Property Use or Function <div style="display: flex; justify-content: space-between;"> <div> Residence <input type="radio"/> Commercial <input type="radio"/> Other <input checked="" type="radio"/> </div> <div> UTM# Zone 17S, 742966.82 m E, 4386911.01 m N </div> </div> Transportation	Photograph 			
Survey Organization & Date WVDOH, October 2022				Quadrangle Name Stotlers Crossroads
Part of What Survey/FR# New Hope Bridge Replacement S233-8/1-3.05				

**Sketch Map of Property
Or Attach Copy of USGS Map**



Site No.

Present Owners State of West Virginia	Owners Mailing Address 1900 Kanawha Blvd Charleston, WV 25305
Describe Setting _____ Acres <u>No</u> Archaeological Artifacts Present New Hope is a rural area of Morgan County, located southwest of Berkely Springs. New Hope Bridge carries one lane of traffic over Sleepy Creek, a tributary of the Potomac River. The surrounding land is mostly forested with some residences set on large parcels.	
Description of Building or Site (Original and Present) _____ Stories _____ Front Bays The New Hope Bridge was constructed in 1916 by the Concrete Steel Bridge Company of Clarksburg. The structure is a two-span, reinforced concrete deck arch bridge supported on full-height reinforced concrete abutments and a solid reinforced concrete pier. The pier has a decorative nose cap, and the solid parapets are incised with a geometric pattern. The overall length of the bridge is 108 feet, 3 inches, while the width is 16 feet, 2 inches.	
Alterations	<div style="display: flex; align-items: center; gap: 20px;"> <div style="text-align: center;"> <input checked="checked" type="checkbox"/> Yes </div> <div style="text-align: center;"> <input type="checkbox"/> No </div> <div> If yes, describe Repairs were made to the bridge by a WVDOH District Five bridge maintenance crew to address separation of the spandrels. Channel beams were bolted to each spandrel wall, and steel angles were bolted to the upstream east abutment and adjacent spandrel for stabilization. Additionally, a portion of fill material was removed, and full width tie bars were installed to help stabilize spandrel walls. </div> </div>
Additions	<div style="display: flex; align-items: center; gap: 20px;"> <div style="text-align: center;"> <input type="checkbox"/> Yes </div> <div style="text-align: center;"> <input checked="checked" type="checkbox"/> No </div> <div> If yes, describe </div> </div>
Describe All Outbuildings N/A	
Statement of Significance (See Continuation Sheet)	
Bibliographical References (See Continuation Sheet)	
<div style="display: flex; justify-content: space-between;"> <div> Form Prepared By: Ahleah Boise </div> <div> Date: October 2022 </div> </div> <div style="margin-top: 10px;"> Name/Organization: WVDOH Address: West Virginia Division of Highways 1900 Kanawha Blvd., Building Five Charleston WV 25305 Phone #: (304) 630-0248 </div>	



West Virginia Division of Culture and History
 State Historic Preservation Office

This program receives federal funds from the National Park Service. Regulations of the U. S. Department of the Interior prohibit unlawful discrimination in department Federally Assisted Programs on the basis of race, color, national origin, age or handicap. Any person who believes he or she has been discriminated against in any program, activity, or facility operated by a recipient of Federal Assistance should write to: Director, Equal Opportunity Program, U.S. Department of the Interior, National Park Service, P.O. Box 37127, Washington, D.C. 20013-7127.

WEST VIRGINIA HISTORIC PROPERTY FORM CONTINUATION SHEET

NAME: New Hope Bridge
SURVEY NUMBER: MN-0232
PROJECT NUMBER: S233-8/1-3.05

Continuation Sheet Date: January 2023

Continuation Sheet prepared by:

Ahleah Boise
Historian
West Virginia Division of Highways
1334 Smith Street
Charleston, WV 25305
304-630-0248

Prepared for:

New Hope Bridge Replacement
State Project S233-8/1-3.05
Morgan County



Location Map:



Setting: This bridge is set in a rural area of Morgan County, carrying one lane of traffic over Sleepy Creek, a tributary of the Potomac River. The surrounding land is mostly forested with some residences set on large parcels.

WEST VIRGINIA HISTORIC PROPERTY FORM

CONTINUATION SHEET

NAME: New Hope Bridge

SURVEY NUMBER: MN-0232

PROJECT NUMBER: S233-8/1-3.05

Description: The New Hope Bridge was constructed in 1916 by the Concrete Steel Bridge Company of Clarksburg. The structure is a two-span, reinforced concrete deck arch bridge supported on full-height reinforced concrete abutments and a solid reinforced concrete pier. The pier has a decorative nose cap, and the solid parapets are incised with a geometric pattern. The overall length of the bridge is 108 feet, 3 inches, while the width is 16 feet, 2 inches.

Repairs were made to the bridge by a WVDOH District Five bridge maintenance crew to address separation of the spandrels. Channel beams were bolted to each spandrel wall, and steel angles were bolted to the upstream east abutment and adjacent spandrel for stabilization. Additionally, a portion of fill material was removed, and full width tie bars were installed to help stabilize spandrel walls.

Statement of Significance: This bridge is located in a rural area of Morgan County known as New Hope. The area is about five miles southwest of Berkeley Springs, the county seat. The New Hope Bridge is located approximately 400 feet southeast of Spohrs Road on Mountain Run Road. This bridge was built by the Concrete Steel Bridge Company. The Concrete Steel Bridge Company based out of Clarksburg was incorporated in 1914 by Frank D. McEnteer and P. M. Harrison. McEnteer was born in Reynoldsville, Pennsylvania and attended Harvard School of Engineering, graduating in 1905. He moved to Clarksburg, West Virginia in 1911, where he was the construction engineer of the Palace Furniture Company building, the first reinforced concrete building in West Virginia. From 1912 until 1914, he advertised his services as a contracting engineer specializing in fire-proof buildings and designs in reinforced concrete.¹

NRHP Criteria Evaluation: Other than a general association with transportation and area history, the New Hope Bridge is not known to be associated with events which have made a significant contribution to broad patterns of the nation's history. It is not related to any of several historical contexts established for Morgan County. It was built in the early 20th century and is not associated with the settlement history, historic roads or turnpikes, or local industries. The bridge is not eligible for listing in the National Register of Historic Places under Criterion A.

The New Hope Bridge is not strongly associated with the lives of persons significant in the past. Frank. Duff McEnteer, an engineer of statewide significance, was the consulting engineer, however the bridge is not uniquely and specifically associated with McEnteer. It is not eligible for listing in the NRHP under Criterion B.

This double arch bridge has a continuous span design, representing an innovation in the engineering of concrete bridges. This bridge was designed and constructed by the Concrete Steel Bridge company of Clarksburg, WV, an engineering firm whose work is distinguishable on the state level. The Concrete Steel Bridge Company was a pioneer in the construction of reinforced concrete bridges in the state and was awarded many projects throughout the state. The company specialized in shorter span bridges of 50 to 100 feet, many of which were single or double arches. It was previously evaluated as eligible for listing in the NRHP under Criterion C and it remains so.

The bridge was built in the 20th century of commonplace materials using common construction techniques. It is not eligible for listing in the NRHP under Criterion D.

⁶ The Daily Telegram, Clarksburg West Virginia, Advertisements and Classifieds, February 8, 1912, et. al.

WEST VIRGINIA HISTORIC PROPERTY FORM

CONTINUATION SHEET

NAME: New Hope Bridge
SURVEY NUMBER: MN-0232
PROJECT NUMBER: S233-8/1-3.05

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Stotlers Crossroads Quadrangle, 1958, 1982; 1:24,000.

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WEST VIRGINIA HISTORIC PROPERTY FORM CONTINUATION SHEET

NAME: New Hope Bridge
SURVEY NUMBER: MN-0232
PROJECT NUMBER: S233-8/1-3.05



Upstream
elevation view of
New Hope Bridge.



Detail of plaque.

WEST VIRGINIA HISTORIC PROPERTY FORM

CONTINUATION SHEET

NAME: New Hope Bridge

SURVEY NUMBER: MN-0232

PROJECT NUMBER: S233-8/1-3.05



Detail of builder's plaque.