

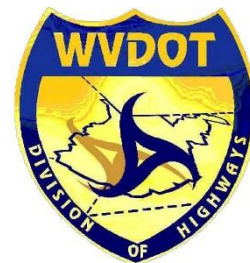
North Whites Addition Arch Bridge (LG-0757)  
Logan County, West Virginia  
State Project #S323-119/26-0.33  
Federal Project #STP-1192(050)D



State Level of Historic Documentation  
Bridge Number: 23-119/26-000.33  
Federal Bridge Number: 00000000023A150

GAI Project Number: C121823.03

February 2018



Prepared by: GAI Consultants, Inc.  
The BB&T Building  
300 Summers Street, Suite 1100  
Charleston, West Virginia 25301

Prepared for: West Virginia Department of Transportation  
Division of Highways  
1334 Smith Street  
Charleston, West Virginia 25301

State Level of Historic Documentation  
North Whites Addition Arch Bridge (LG-0757)  
Logan County, West Virginia

State Project #S323-119/26-0.33  
Federal Project #STP-1192(050)D  
Bridge Number: 23-119/26-000.33  
Federal Bridge Number: 00000000023A150

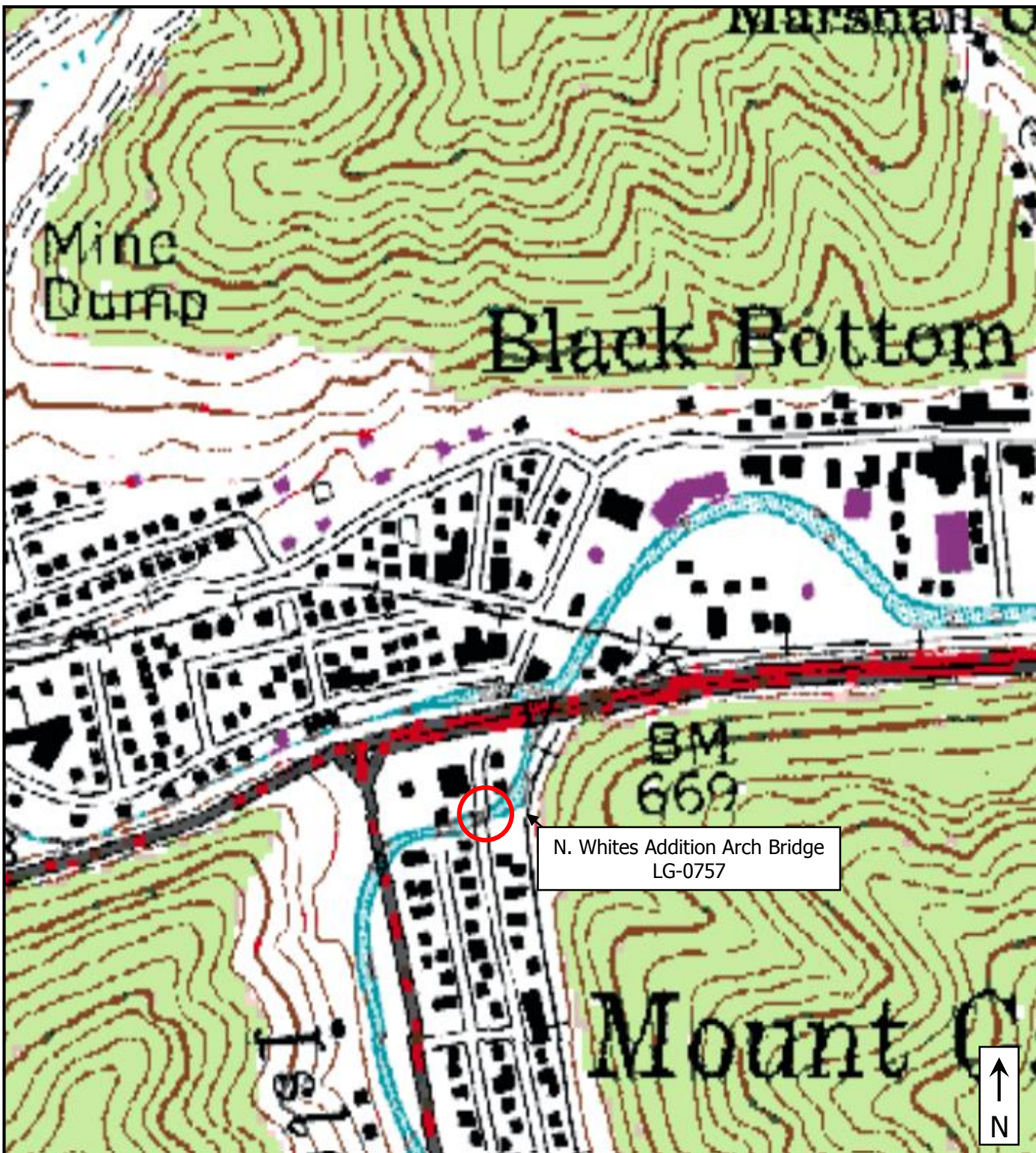
GAI Project Number: C121823.03

February 2018

Prepared for:  
West Virginia Department of Transportation  
Division of Highways  
1334 Smith Street  
Charleston, West Virginia 25301

Prepared by:  
Amanda K. Stander, Architectural Historian  
GAI Consultants, Inc.  
The BB&T Building  
300 Summers Street, Suite 1100  
Charleston, West Virginia 25301





SOURCE: UNITED STATES GEOLOGICAL SURVEY, HOLDEN, WV QUADRANGLE, 7.5 MINUTE SURVEY, 1963, PHOTOREVISED 1989.



Resource Location

USGS MAP  
N. WHITES ADDITION ARCH BRIDGE (LG-0757)  
MOUNT GAY, LOGAN COUNTY



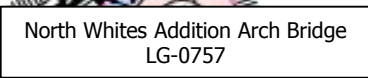
gai consultants

MT. GAY/NORTH WHITES ADDITION BRIDGE  
REPLACEMENT PROJECT

DRAWN: AKS  
CHECKED: TDB

DATE: 05/25/2016  
APPROVED: JG





DATE: 02/07/2018  
APPROVED: TDB





SOURCE: GOOGLE EARTH, 2016.



Resource Location

MODERN AERIAL  
N. WHITES ADDITION ARCH BRIDGE (LG-0757)  
MOUNT GAY, LOGAN COUNTY



MT. GAY/NORTH WHITES ADDITION BRIDGE  
REPLACEMENT PROJECT

DRAWN: AKS  
CHECKED: TDB

DATE: 05/25/2016  
APPROVED: JG





SOURCE: WV FLOOD TOOL SUPPORTED  
BY FEMA, WV NFIP, AND WV GIS  
TECHNICAL CENTER, SEPT 2016

FLOOD MAP  
N. WHITES ADDITION ARCH BRIDGE (LG-0757)  
MOUNT GAY, LOGAN COUNTY



MT. GAY/NORTH WHITES ADDITION BRIDGE  
REPLACEMENT PROJECT

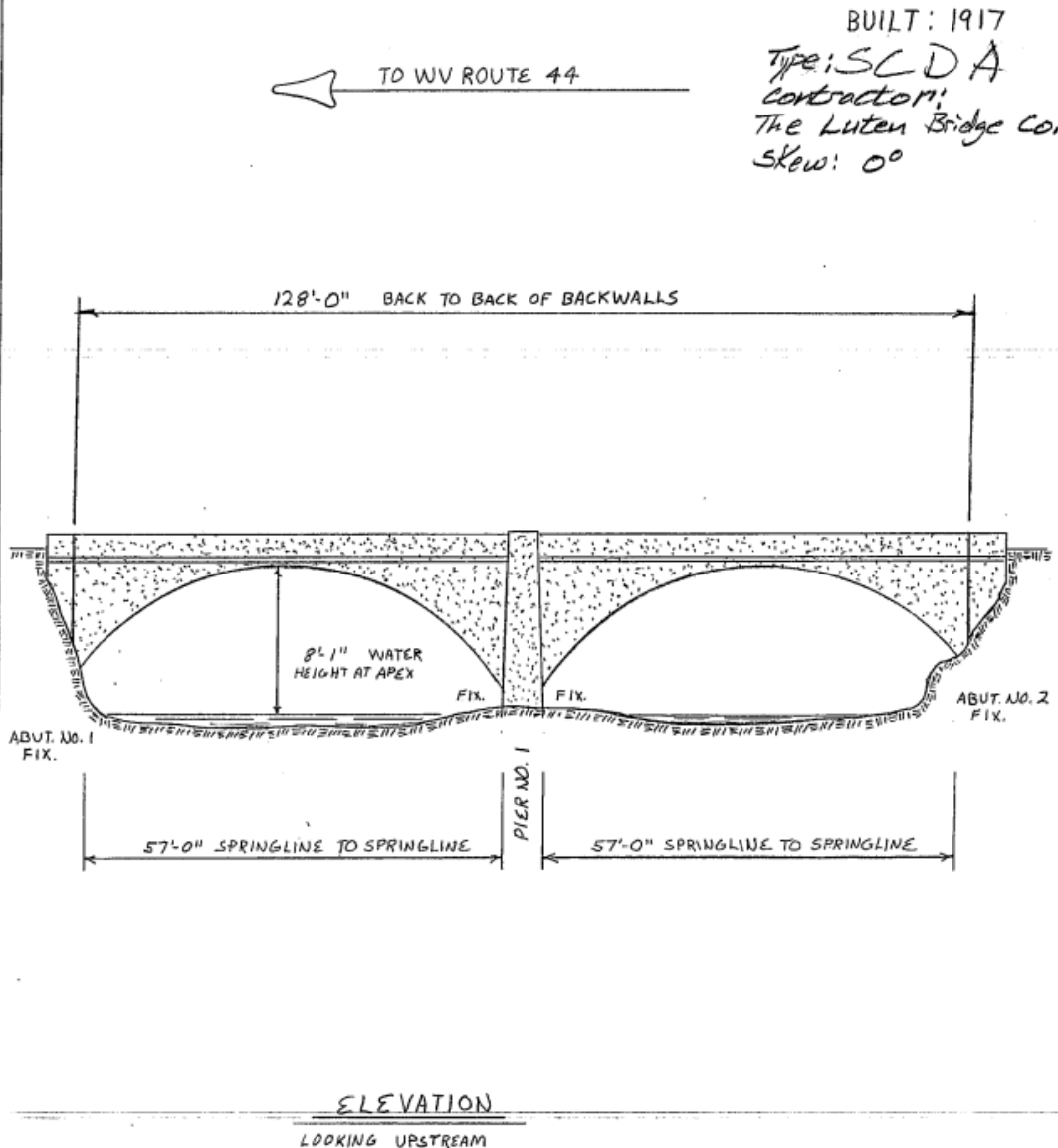
DRAWN: AKS  
CHECKED: TDB

DATE: 02/07/2018  
APPROVED: TDB

# THE WEST VIRGINIA DEPARTMENT OF HIGHWAYS

## STRUCTURES DIVISION

BY: <i>Mark A. Jordan</i> CHECKED: <i>IDW</i> ORGANIZATION: 0298	DATE: 7-9-86 DATE: 4/94	BRIDGE PROFILE	BRIDGE NO. 23-119/26-0.33 COUNTY LOGAN DISTRICT TWO	SHEET 2 OF 15
--	----------------------------	----------------	---	---------------------



SOURCE: West Virginia Division of Highways, 1998, Bridge Inspection Report, North Whites Addition Arch Over Island Creek, County Route 119/26 (SLS), Logan County, District Two, Non-NHS, A.D.T. 2,000 - Year 1986. Revised April, 1996

INSPECTION DRAWING  
N. WHITES ADDITION ARCH BRIDGE (LG-0757)  
MOUNT GAY, LOGAN COUNTY



MT. GAY/NORTH WHITES ADDITION BRIDGE  
REPLACEMENT PROJECT

DRAWN: AKS  
CHECKED: TDB

DATE: 02/07/2018  
APPROVED: TDB

## **WEST VIRGINIA HISTORIC BRIDGE RECORDATION**

North Whites Addition Arch Bridge (LG-0757)

Bridge Number: 23 – 119/26 – 000.33

Federal Bridge Number: 0000000023A150

Location: North Whites Addition Arch Bridge carrying Riverview Avenue (CR 119/26), spanning Island Creek, Cherry Tree (Mount Gay/Shamrock), Logan County, West Virginia.

USGS Quadrangle: Holden, West Virginia [7.5-Minute Series, 1963 (Photorevised 1989)]

UTM Coordinates: 17/411235.59 m E/41899124.72 m N

Date of Construction: 1917

Engineer: Daniel B. Luten

Builder: Luten Bridge Company, York, Pennsylvania

Present Owner: West Virginia Department of Transportation  
Division of Highways  
1334 Smith Street  
Charleston, WV 25301

Present Use: Vehicular Bridge. Scheduled for removal and replacement.

Significance: The North Whites Addition Arch Bridge is historically and technologically significant as an example of the work of nationally-recognized bridge engineer, Daniel B. Luten. Constructed by the Luten Bridge Company, the bridge was constructed in 1917 and spans Island Creek. The 128-foot-long bridge contains a 17-foot, 8-inch-wide concrete deck with concrete parapets decorated with incised rectangles. The bridge is supported by concrete abutments with two 57-foot spans. The bridge possesses Luten's distinctive rounded arch ring details and nonsymmetrical design in what became known as the Luten Arch, a patented design significant for its innovative use of reinforcements used to alleviate tension and load, creating a significantly lighter bridge.

Project Information: This documentation was conducted on behalf of the West Virginia Division of Highways (WVDOT) owing to the poor condition of the bridge structure. Future deterioration of the bridge would likely lead to bridge closure; therefore, the existing structure warrants replacement. The recordation was conducted in May 2016 in accordance with a Memorandum of Agreement between the West Virginia Department of Transportation and the West Virginia State Historic Preservation Office. These measures are required prior to the replacement of this National Register-eligible structure. The project was completed as part of the Mt. Gay Bridge/North Whites Addition Bridge Replacement Project.

Amanda K. Stander, AICP, Architectural Historian  
GAI Consultants, Inc.  
The BB&T Building  
300 Summers Street, Suite 1100  
Charleston, West Virginia 25301



## Summary Description of Bridge and Setting

The North Whites Addition Arch Bridge carries Riverview Avenue (CR 119/26) over Island Creek and is situated on the western outskirts of the City of Logan, Logan County, West Virginia. The bridge spans the fork in a north-south direction and provides vehicular access to the community of Black Bottom to the north, and Mount Gay and Cherry Tree to the south.

The North Whites Addition Bridge is located north of the once bustling community of Mount Gay, a small mining town located approximately 0.95-mile southwest of the City of Logan. Mount Gay thrived in the early-twentieth century during the height of the coal era in Logan County and West Virginia. The majority of the extant structures in Mount Gay and Cherry Tree are late-nineteenth and early-twentieth century frame residential and masonry commercial buildings concentrated in an area south of the Mt. Gay Deck Arch Bridge, on the east and west sides of Riverview Avenue. According to historic-era photographs, the area surrounding the North Whites Addition Bridge was once a bustling area of commercial buildings known as Monitor Junction. Few historic-era commercial buildings are extant in the immediate vicinity of the bridge, having been demolished for the construction of the Mt. Gay Overpass.

The Mt. Gay Overpass (LG-0749/Bridge No. 23-073/00-002.31) was constructed in 1956 and spans Riverview Avenue and a portion of the North Whites Addition Bridge, which has significantly diminished its integrity of setting. A trestle (LG-0086) carries the former Chesapeake and Ohio (current CSX) Railroad (C&O) over Riverview Avenue, approximately 500 feet northeast of the bridge. Non-historic commercial infill comprises the majority of the area, which has also diminished the integrity of the bridge's setting. The North Whites Addition Bridge was determined to be eligible for listing in the National Register of Historic Places (NRHP) under Criterion C in 2013; this determination was concurred by the West Virginia Division of Culture and History. It should be noted that no other information was uncovered to suggest the North Whites Addition Bridge meets any other NRHP eligibility criteria.

The surrounding area consists of the unincorporated community of Cherry Tree, a former coal mining hamlet associated with numerous coal mining operations that existed in the vicinity during the coal mining industry's boom years in the late-nineteenth through mid-twentieth century. The area to the south is characterized by many frame dwellings, typically one-story, built in the vernacular styles common to the coal heritage region, as well as a handful of masonry commercial and industrial buildings. Primarily vacant land occupies the area to the north which is bisected by the C&O Railroad and trestle.

The landscape consists of steeply-wooded mountainsides extending to Mt. Gay summit creating a narrow valley floor adjacent to Island Creek. It is on this landscape that the community has been built similar to so many others in the coal heritage region of southern West Virginia. Because of the isolation of communities to the south, the North Whites Addition Bridge represents an important link to the more populous areas to the north which includes the county seat of Logan. Thus the bridge is very closely tied to the history of this small village and those located immediately to the south. Though many of Cherry Tree's structures survive from its origins in the early-twentieth century, the character of the community has been modified significantly due to numerous modern and intrusive highway projects. These include the creation of SR 44, which bypasses the small hamlet of Cherry Tree to the west resulting in the loss of numerous residences. Moreover, in 1954 US 119 was reconfigured creating the Mt. Gay overpass, which passes almost directly over the North Whites Addition Arch Bridge.

The one-lane, two-span North Whites Addition Arch Bridge was built in 1917 and measures 128'-0" in length and 17'-8" wide from parapet to parapet. The bridge is constructed on a curved section of roadway and is not skewed, with the approach from Abutment No. 1 from a slightly curved section of roadway and the approach at Abutment No. 2 from a moderately curved section of roadway (WVDOH 1998). The bridge has no clearance restrictions, and sits approximately 8'-1" above the waters of

Island Creek at the apex of each arch. It contains two simple reinforced concrete spandrel-filled parabolic deck spans each measuring 57'-0" in length, springline to springline. It is supported at each end by concrete abutments and intermediately by a solid concrete pier. The foundation material of the substructure units is unknown. Each arch barrel is constructed of thick reinforced concrete and spans 17'-8" wide overall, with a 10-inch-thick wearing surface of asphalt on top of dirt fill. The roadway width is 16'-3", and there are no sidewalks, railings, or curbs on the structure. Concrete parapets with carved rectilinear decorative elements are present, however, which contribute to the overall aesthetics of the bridge. It appears discoloration on the concrete of the east parapet is from a former bronze plaque and date stone, which was likely identical to that of the twin Mt. Gay Deck Arch Bridge (LG-0758/LG-0764) located approximately 0.07-mile to the north. It reads "1917, Luten Bridge Company, York, Pa." Although there has been no record of any maintenance work performed on the structure, iron S-hooks attached to the parapet on its east side hold chains supporting a polyvinyl chloride drainage pipe. Older iron brackets attached at regular intervals on the east side of the parapet (near the road decking) may suggest earlier pipes to drain to runoff from the bridge deck.

The bridge is significantly deteriorated; the one-lane design is not sufficient to carry current traffic loads. Erosion was observed along concrete support arches, particularly along the bridge's east elevation and a moderate amount of debris (from upstream Island Creek) has collected beneath the piers. There is a significant amount of vegetative growth on the embankment surrounding the bridge.

The bridge is significant due to its association with the development of the Luten Arch, with this being a relatively early example of the application of his 1900 patent. The Luten Arch was an important advancement in the design and engineering structure of bridges, and the North Whites Addition Arch Bridge is unique in that it is one of a set of "twin" paired bridges crossing Island Creek, with an identical structure (Mt. Gay Deck Arch Bridge) (LG-0758/LG-0764) directly to the north crossing Cooperas Mine Fork. Daniel B. Luten's use of the transverse and reinforced concrete allowed his bridges to be lighter, and he obtained more than 30 patents in his lifetime. By 1919, Luten, who was born in Grand Rapids, Michigan, and based in Indianapolis, Indiana, claimed to have designed approximately 17,000 arches, with examples of the design being found in all but three states. Luten graduated from the University of Michigan in 1894 with a degree in civil engineering, and his Luten Bridge Company maintained offices in York, Pennsylvania; Clarksburg, West Virginia; Huntington, West Virginia; Atlanta, Georgia; and Palatka, Florida. His pioneering designs are significant as they are still adapted and in use today.

## **Summary History of Area and North Whites Addition Arch Bridge**

Logan County, West Virginia was formed in 1824 and named in honor of the famous Mingo chief. The Village of Logan was first established as Lawnsville in 1827 with the erection of the Logan County Courthouse, but was incorporated in 1852 with the name of Aracoma (Spence 1976). Its first mayor was Thomas Dunn English who wrote the famous poem "Ben Bolt" in 1843 (Logan County, WV History and Nostalgia 2016a). On June 20, 1863 West Virginia seceded from the Confederate State of Virginia and joined the Union. The Aracoma name was changed to Logan in 1907.

The first coal mine in the region was opened in 1904 by Harry S. Gay, Sr., the result of many efforts and enterprise of three generations of his family (Logan County, WV History and Nostalgia 2016b). Gay himself was trained as a mining engineer, and was the grandson of Samuel Gay. Samuel Gay came to the coal fields of southern West Virginia early on with William McQuail, where for a time they operated under the name of the Turkey Gay Coal Company in the Pocahontas District in Mercer and McDowell Counties, southeast of Logan (Logan County, WV History and Nostalgia 2016b). H.S. Gay, Sr., however, was the true pioneer in the Logan region. He was born in Mount Carmel, Pennsylvania and trained in the technical fields at Lafayette College. He later worked as a mining engineer in the anthracite fields of northeastern Pennsylvania and New Jersey. He first arrived in Logan County in 1903 in order to prepare a report on the coal fields for a coal company in Shamokin, Pennsylvania. While



there he located the Monitor-Yuma Land lease and acquired a lease of 800 acres, which later grew to become the Gay Coal and Coke Company (Logan County, WV History and Nostalgia 2016a). He shipped the first car load of coal on Thanksgiving Day in 1904 (Atkins 2013).

As with much of Logan County, the history of the Mt. Gay hamlet of Cherry Tree is tied to coal. Cherry Tree was originally laid out as a town in 1905 by W. P. Henritze; it was known as the Henritze Addition to the Town of Logan (Atkins 2013). Henritze anticipated the need for quality housing as part of the booming coal industry, as Henry S. Gay had just established his Gay Coal and Coke Company operations in this section of the Island Creek valley in 1904. At nearly the same time, in 1902, William H. Coolidge and Albert F. Holden bought a substantial tract of 30,000 acres on the Cooperas Fork of Island Creek with the intent of establishing a significant coal mining enterprise, later called the Island Creek Coal Company (Dobson, Stone, and Valente 1991; West Virginia Encyclopedia 2016a). They built a railroad line from Logan which later became part of the Chesapeake and Ohio Railroad (C&O), which at the time had just completed its Guyan Valley Extension (West Virginia Encyclopedia, 2016b). As a result, Mt. Gay had important rail access to Logan and hence to wider markets via the C&O network. The name of "Cherry Tree," or "Cherry Tree Bottom" was the common name given to the Henritze Addition, particularly the upper section, due to the existence of several large cherry trees at the outskirts of the settlement (Atkins 2013). When the town was first established, a man by the name of White had a farm on the south side of Island Creek, which later became known as "White's Addition." The original thoroughfare, now Riverview Avenue, was called Cherry Street, and an early local name for the lower section of the village was "Lower Ram Cat." The back alley that runs parallel to the main thoroughfare was often called "Ram Cat Alley" (Atkins 2013).

Due to its proximity to both the Island Creek and Henry S. Gay holdings, Cherry Tree experienced a building boom during the first two decades of the twentieth century. By 1930, Cherry Tree had its own primary school, church, four grocery stores, a bakery, a gas station, and several dozen residences, in addition to numerous other small business enterprises. The Logan Baking Company, which was later renamed Butter Crust Bread, began as a small enterprise founded by Italian immigrants Dante Belladonna, Patsy Ferzacci, and Miller Farley. It later grew into a significant employer for the small town (Atkins 2013). Their "butter crust bread" was well-known and was distributed throughout West Virginia. It was eventually bought by the Sunbeam Bread Company, and later became home to National Cable Repair. R.C. Cola also had a bottling plant, operated by a man named Hill Rigdon, on the east side of Riverview, directly south of the North White's Addition Bridge; this structure is no longer extant (Davidson and McCormack 2012).

The primary house of worship, Pilgrim Holiness Church, was a religious denomination associated with the holiness movement that split from the Methodist Episcopal Church in 1897. It was first organized in Cincinnati, Ohio as the International Holiness Union and Prayer League and was rechristened the Pilgrim Holiness Church in 1922. A movement to establish a branch in Cherry Tree began with spirited tent revival meetings in the first decade of the twentieth century followed by the raising of the church circa 1920 (McCormack 2012). The congregation grew to include a nearby church camp in the mountains. The organization eventually merged with the Wesleyan Methodists in 1968 to form the Wesleyan Church. Despite the town's somewhat isolated location in the flat bottom between steep mountains, it did have street lights and sidewalks prior to 1930, although the roads remained unpaved for many years. The town continued to serve as a predominantly coal mining town, and was home to several interesting individuals, including Red Berry, said to be the brother of the famous musician Chuck Berry, and many immigrant families from southern and eastern Europe (Davidson and McCormack 2012).

The history and heritage of Cherry Tree and Logan County remained intimately connected with coal for many years. Logan County witnessed one of the most famous coal mining labor incidents, the 1921 Blair Mountain Battle (West Virginia Encyclopedia, 2016b). This was one of the largest armed uprisings since the American Civil War. The murder of Sid Hatfield, who had become a hero to coal miners when

he stood up to the Baldwin Felts Mine Guards during the 1920 Matewan Massacre, incensed coal miners across the state of West Virginia and resulted in the uprising of more than 10,000 armed miners (West Virginia Encyclopedia 2016b). These groups marched to free Mingo County from what they viewed as oppressive control by coal companies. The force was met by a heavily armed contingent supported by coal companies on the Logan County line. This battle continued for several days until the U.S. Military eventually stopped the marching miners.

Logan County is also home to another infamous mining incident, the Buffalo Creek Disaster. The Buffalo Creek Flood accident that occurred on February 26, 1972 when a coal slurry impoundment dam built on a hillside in Logan County (by the Pittston Coal Company) burst four days after being declared 'satisfactory' by a federal mine inspector (Logan County, WV History and Nostalgia 2016b). The resulting flood unleashed approximately 132 million gallons of black waste water upon the residents of 16 coal mining communities in Buffalo Creek Hollow. Out of a population of 5,000 people, 125 people were killed, 1,121 were injured and over 4,000 were left homeless. A total of 507 houses were destroyed in addition to 44 mobile homes and 30 businesses (Logan County, WV History and Nostalgia 2016b). The incident completely leveled the town of Saunders, West Virginia.

Unfortunately, with the slow demise of the coal industry beginning after World War II, Cherry Tree began to see a gradual decline in its population and businesses. This was made worse by the expansion of SR 44, along with the eventual widening and reconfiguring of U.S. 119 in the 1950s. The new SR 44 resulted in the demolition of an entire row of residences in Cherry Tree as well as the elimination of the original primary school house. As a result, the small hamlet began a slow decline into obscurity. Today, it is a shadow of its former self, with many of the frame dwellings in dilapidated or deteriorated condition and many of the businesses gone.



## Sources

Atkins, Eddie

2013 "History of Cherry Tree." <http://wp.cherrytreewv.com/2013/06/notes-from-eddie-atkins/>

Davidson, Bruce, and Robert McCormack.

2012 "Map of Cherry Tree / Residents." <http://wp.cherrytreewv.com/2012/05/cherry-tree-map-residents/>

Dobson, Cindy, Stacy Stone, and Kim Valente

1991 "Coal Heritage Survey: Southern West Virginia Reconnaissance Study of Sites Related to the History of Coal Mining." *Phase I Final Report: Boone, Mingo, Wyoming, McDowell and Logan Counties. 8 November 1991.* West Virginia State Historic Preservation Office, Charleston, WV.

Logan County, WV, History and Nostalgia

2016a Harry S. Gay, Jr. – Gay Coal and Coke Company. <http://loganwv.us/harry-s-gay-jr/>

2016b The Buffalo Creek Disaster. <http://loganwv.us/the-buffalo-creek-disaster/>

McCormack, Robert.

2012 "Pilgrim Holiness Church." <http://wp.cherrytreewv.com/2012/05/pilgrim-holiness-church-cherry-tree-wv/>

Spence, Robert Y.

1976 *The Land of the Guyandot.* Harlo Press, Detroit, MI.

United States Geological Survey

2016 USGS Historical Topographic Map Explorer. <http://historicalmaps.arcgis.com/usgs>.

West Virginia Division of Highways (WVDOH)

1998 *Bridge Inspection Report*, Mt. Gay Deck Arch Over Mud Fork, County Route 119/26 (SLS), 0.03 Mile West of County Route 5, Logan County, District Two, Non-NHS, A.D.T. 2,000 – Year 1989. Revised July 23, 1998.

West Virginia Encyclopedia


2016a "Island Creek Coal Company." <http://www.wvencyclopedia.org/articles/914>

2016b "Battle of Blair Mountain." <http://www.wvencyclopedia.org/articles/532>



Internal Rating: \_\_\_\_\_

## WEST VIRGINIA HISTORIC PROPERTY INVENTORY FORM

<b>Street Address</b>	<b>Common/Historic Name/Both</b>	<b>Field Survey #</b>	<b>Site # (SHPO Only)</b>
<b>Town or Community</b>	<b>County</b>	<b>Negative No.</b>	<b>NR Listed Date</b>
<b>Architect/Builder</b>	<b>Date of Construction</b>	<b>Style</b>	
<b>Exterior Siding/Materials</b>	<b>Roofing Material</b>	<b>Foundation</b>	
<b>Property Use or Function</b> Residence <input type="radio"/> Commercial <input type="radio"/> Other <input type="radio"/>	<b>UTM#</b>	 <p style="font-size: small;">Photograph 1. LG-0757, N. Whites Addition Arch Bridge, Facing South.</p>	
<b>Survey Organization &amp; Date</b>	<b>Quadrangle Name</b>		
	<b>Part of What Survey/FR#</b>		

Sketch Map of Property  
Or Attach Copy of USGS Map

Site No.

N



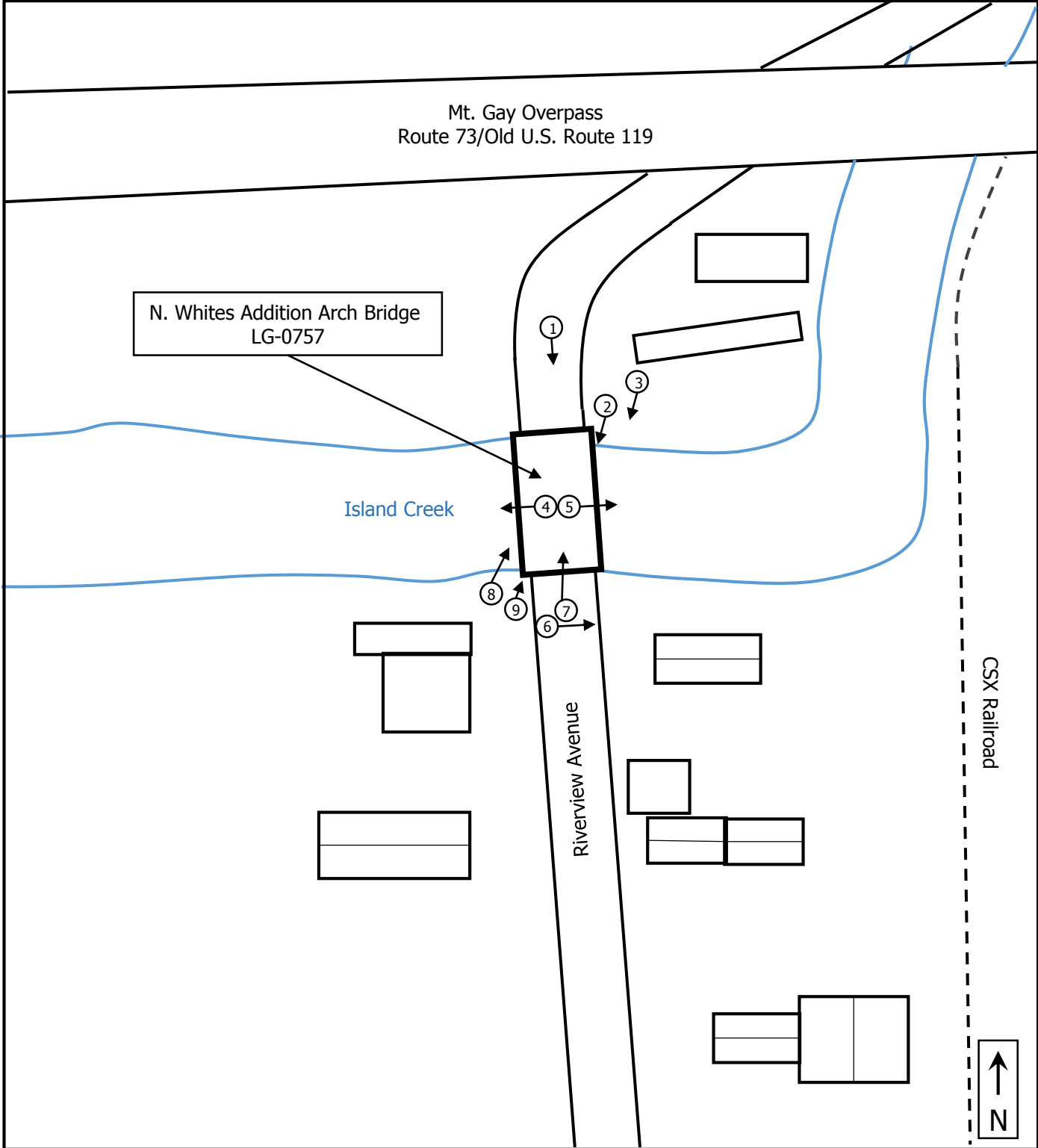


<b>Present Owners</b>  <b>Phone #</b>	<b>Owners Mailing Address</b>								
<b>Describe Setting</b>  <div style="text-align: right;">             _____ Acres              _____ Archaeological              Artifacts Present           </div>									
<b>Description of Building or Site (Original and Present)</b> <div style="text-align: right; margin-top: -20px;">             _____ Stories    _____ Front Bays           </div> <div style="text-align: right; margin-top: 20px;"> <i>(Use Continuation Sheets)</i> </div>									
<table style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 20%;"><b>Alterations</b></td> <td style="width: 10%;"></td> <td style="width: 10%;"><b>If yes, describe</b></td> <td style="width: 60%;"></td> </tr> <tr> <td style="text-align: center;">Yes</td> <td style="text-align: center;">No</td> <td></td> <td></td> </tr> </table>		<b>Alterations</b>		<b>If yes, describe</b>		Yes	No		
<b>Alterations</b>		<b>If yes, describe</b>							
Yes	No								
<table style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 20%;"><b>Additions</b></td> <td style="width: 10%;"></td> <td style="width: 10%;"><b>If yes, describe</b></td> <td style="width: 60%;"></td> </tr> <tr> <td style="text-align: center;">Yes</td> <td style="text-align: center;">No</td> <td></td> <td></td> </tr> </table>		<b>Additions</b>		<b>If yes, describe</b>		Yes	No		
<b>Additions</b>		<b>If yes, describe</b>							
Yes	No								
<b>Describe All Outbuildings</b>   <div style="text-align: right; margin-top: 20px;"> <i>(Use Continuation Sheets)</i> </div>									
<b>Statement of Significance</b>    <div style="text-align: right; margin-top: 20px;"> <i>(Use Continuation Sheets)</i> </div>									
<b>Bibliographical References</b>   <div style="text-align: right; margin-top: 20px;"> <i>(Use Continuation Sheets)</i> </div>									
<table style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 60%;"><b>Form Prepared By:</b></td> <td style="width: 40%;"><b>Date:</b></td> </tr> <tr> <td colspan="2"><b>Name/Organization:</b></td> </tr> <tr> <td colspan="2"><b>Address:</b></td> </tr> <tr> <td colspan="2"><b>Phone #:</b></td> </tr> </table>		<b>Form Prepared By:</b>	<b>Date:</b>	<b>Name/Organization:</b>		<b>Address:</b>		<b>Phone #:</b>	
<b>Form Prepared By:</b>	<b>Date:</b>								
<b>Name/Organization:</b>									
<b>Address:</b>									
<b>Phone #:</b>									



West Virginia Division of Culture and History  
 State Historic Preservation Office

This program receives federal funds from the National Park Service. Regulations of the U. S. Department of the Interior prohibit unlawful discrimination in department Federally Assisted Programs on the basis of race, color, national origin, age or handicap. Any person who believes he or she has been discriminated against in any program, activity, or facility operated by a recipient of Federal Assistance should write to: Director, Equal Opportunity Program, U.S. Department of the Interior, National Park Service, P.O. Box 37127, Washington, D.C. 20013-7127.



LEGEND



Photo Location

SITE PLAN

N. WHITES ADDITION ARCH BRIDGE (LG-0757)  
MOUNT GAY, LOGAN COUNTY



gai consultants

MT. GAY/NORTH WHITES ADDITION BRIDGE  
REPLACEMENT PROJECT

DRAWN: AKS  
CHECKED: TDB

DATE: 05/25/2016  
APPROVED: JG

STATE LEVEL HISTORIC DOCUMENTATION  
INDEX TO PHOTOGRAPHS

North Whites Addition Arch Bridge (LG-0757)

Logan County, West Virginia

State Project #S323-119/26-0.33

Federal Project #STP-1192(050)D

Bridge Number: 23-119/26-000.33

Federal Bridge Number: 00000000023A150

Photographer: Elizabeth Williams, May 2016

- Photograph 1. North Whites Addition Arch Bridge, (LG-0757), Facing South
- Photograph 2. North Whites Addition Arch Bridge, (LG-0757), Detail, Facing Southwest
- Photograph 3. North Whites Addition Arch Bridge, (LG-0575), Facing Southwest
- Photograph 4. North Whites Addition Arch Bridge, (LG-0575), Interior of Parapet 1, Facing West
- Photograph 5. North Whites Addition Arch Bridge, (LG-0575), Interior of Parapet 2, Facing East
- Photograph 6. North Whites Addition Arch Bridge, (LG-0575), Modern Mile Marker
- Photograph 7. North Whites Addition Arch Bridge, (LG-0575), Facing North
- Photograph 8. North Whites Addition Arch Bridge, (LG-0575), Detail, Facing Northeast
- Photograph 9. North Whites Addition Arch Bridge, (LG-0575), Facing Northeast

No original plans exist for this bridge.





**Photograph 1. North Whites Addition Arch Bridge, (LG-0757), Facing South**



**Photograph 2. North Whites Addition Arch Bridge, (LG-0757), Detail, Facing Southwest**





**Photograph 3. North Whites Addition Arch Bridge, (LG-0757), Facing Southwest**



**Photograph 4. North Whites Addition Arch Bridge, (LG-0757), Interior of Parapet 1, Facing West**





**Photograph 5. North Whites Addition Arch Bridge, (LG-0757), Interior of Parapet 2, Facing East**



**Photograph 6. North Whites Addition Arch Bridge, (LG-0757), Modern Mile Marker**





**Photograph 7. North Whites Addition Arch Bridge, (LG-0757), Facing North**



**Photograph 8. North Whites Addition Arch Bridge, (LG-0757), Detail, Facing Northeast**





**Photograph 9. North Whites Addition Arch Bridge, (LG-0757), Facing Northeast**