

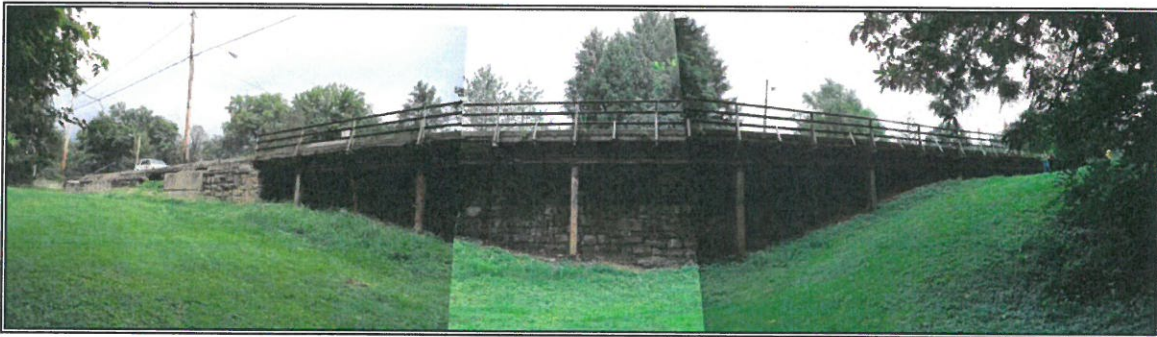
*Cantilevered Sidewalk Segment and
Stone Retaining Wall & Culvert Drainage System*

Ohio County

State Project #: S335-40-3.44 (Modification)

Federal Project #: N/A

FR#: 16-533-OH



STATE LEVEL OF HISTORIC DOCUMENTATION

May 2018


***West Virginia
Department of Transportation
Division of Highways
Engineering Division
Environmental Section
(304) 558-2885***

National Road/US40 Sidewalk Replacement

*Map showing location of sidewalk replacement along National Road/US40 Rehabilitation Project (S335-40-3 44) (FR#: 18-533-OH).



Legend

 Nat'l Rd, US 40

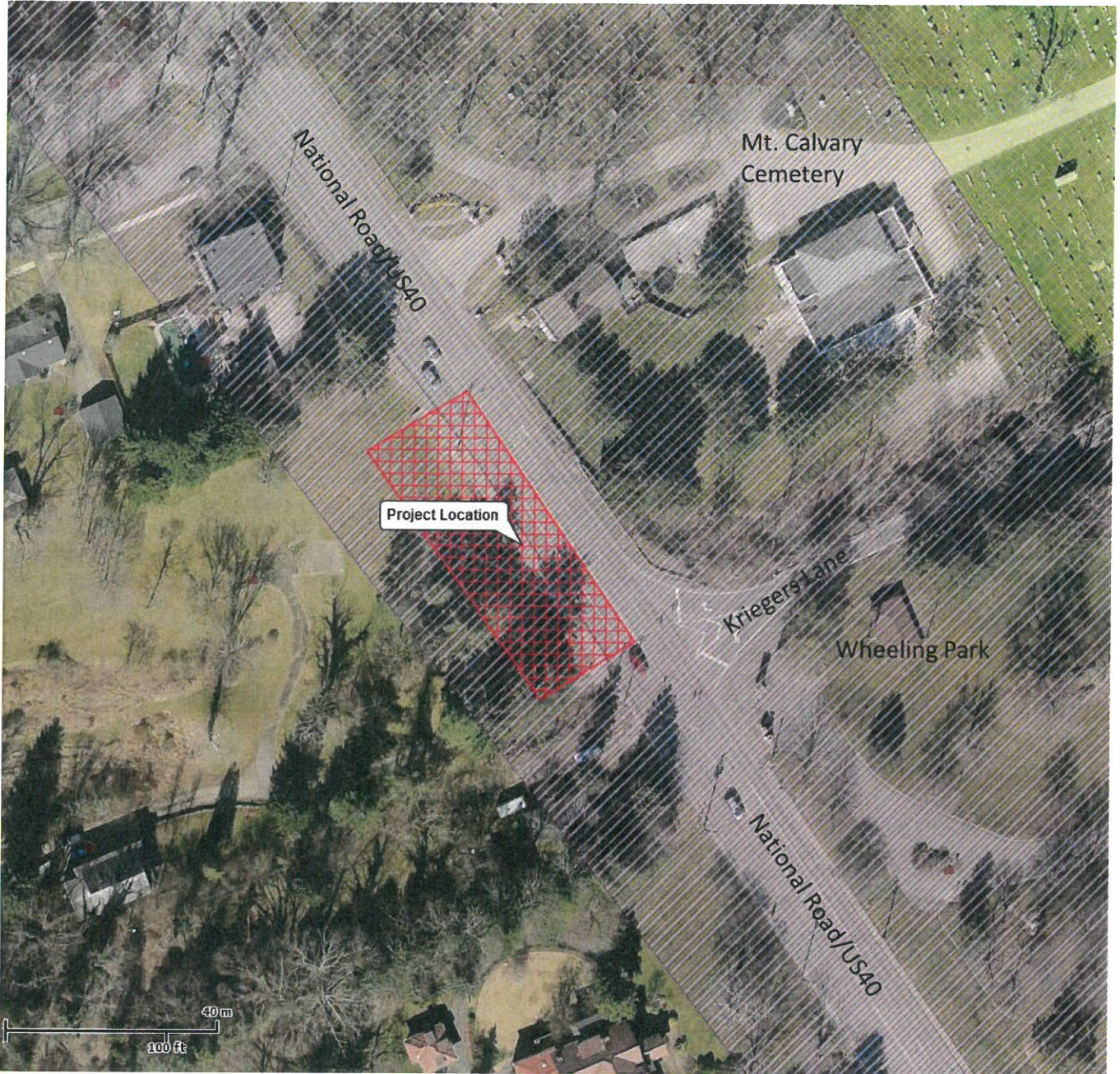
National Road Sidewalk Replacement

6

Google Earth



National Road/US40 Sidewalk Replacement



STATE LEVEL HISTORIC DOCUMENTATION
CANTILEVERED SIDEWALK SEGMENT AND
STONE RETAINING WALL & CULVERT DRAINAGE SYSTEM

Location: US Route 40/ National Road
Wheeling (east of downtown)
Ohio County
West Virginia

USGS Wheeling Quadrangle

Date of Construction: 1926 (Sidewalk)/ c.1900 (Stone Wall)

Builder: WV State Road Commission (Sidewalk)/ Unknown (Stone Wall)

Present Owner: West Virginia Department of Transportation
Division of Highways
1900 Kanawha Boulevard, Building 5, Room A-110
Charleston, WV 25305

Present Use: Roadway support and pedestrian walkway.

Significance: The Cantilevered Sidewalk Segment and Stone Retaining Wall & Culvert Drainage System are significant as contributing structures to the National Road Corridor Historic District.

Project Information: The project has been undertaken because it was discovered that a portion of the sidewalk along the National Road/US40 is failing and in need of removal and replacement. The project includes removing the failing portion of the existing sidewalk, adding an area of fill next to the roadway for stabilization (covering the stone wall), and constructing a new sidewalk on top of the fill material. The documentation was undertaken in March 2017 in accordance with a Memorandum of Agreement among the West Virginia Department of Transportation and West Virginia State Historic Preservation Office. These measures are required prior to removal/ replacement of these National Register eligible structures.

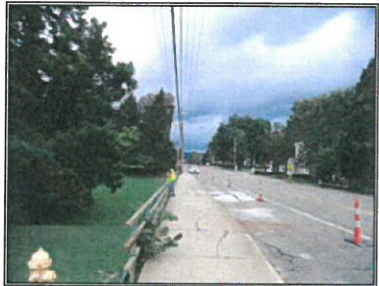
Kristin D. Kraus, Historian
West Virginia Division of Highways
Charleston, WV 25301
March 17, 2017

The Cantilevered Sidewalk Segment and Stone Retaining Wall & Culvert Drainage System are located east of downtown Wheeling in Ohio County on National Road/US40 at milepost 5.41. The National Road was completed from Cumberland, Maryland to Wheeling, (West) Virginia in 1818 and originally passed through rural farmland with occasional taverns and inns for travelers. Near the turn of the 20th century, the area immediately east of Wheeling began to develop as farms were divided up and residential communities were created along the National Road. This increase in traffic led to the need for roadway improvement and was likely a factor in the construction of the stone retaining wall and culvert drainage system c.1900. Eventually, congress passed a federal aid road act and numerous additional roadway improvements were made from 1915-1935, including the construction of the cantilevered sidewalk in 1926.



The Stone Retaining Wall & Culvert Drainage System was constructed c.1900 by an unknown builder. The original stone wall on the eastern side of the roadway is no longer visible as there is a steel and timber retaining wall in its place. The remaining stone wall on the western side is constructed of rough-cut sandstone blocks laid in an irregular course. The wall is approximately 225' long with a height ranging from .5' to 13', depending on the surrounding topography. Areas of concrete reinforcement and a set of concrete stairs have been added to the northern end of the stone wall. The support system for the cantilevered sidewalk was added to the wall in 1926 when the sidewalk was constructed. In 1983, the southern end of the wall was covered by fill material. The stone arch culvert no longer extends under the full width of the roadway, but exists as an approximately 8' x 7' inlet on the eastern side of the roadway. The remainder of the drainage system now consists of a 6' diameter

concrete pipe and a 6' diameter corrugated metal pipe, extending under the roadway and beyond the stone wall to an inlet.



The Cantilevered Sidewalk Segment was constructed in 1926 by the WV State Road Commission. It is located on the western side of the roadway and cantilevers over the stone wall below. It is approximately 133' long and constructed of a concrete surface supported by horizontal metal I-beams tied-in to the stone wall. The sidewalk has a metal railing attached to the supporting horizontal metal beams, which extend beyond the edge of the concrete surface. An additional set of horizontal beams and vertical posts that extend to the ground below were added at a later date to offer extra support. The width of the sidewalk has also been increased so that the sidewalk now abuts the roadway and the original fence separating it from the roadway curb has been removed.

The sidewalk has been reinforced with additional metal beam and post support and the stone retaining wall has been reinforced with concrete and fill material over the years. However, the sidewalk is still failing and poses a safety risk for local pedestrians. Removal and replacement of the sidewalk is necessary to prevent further structural issues.

The Cantilevered Sidewalk Segment and the Stone Retaining Wall & Culvert Drainage System are eligible for the National Register of Historic Places as contributing structures to the National Road Corridor Historic District, which was listed on the National Register in 1993. These structures were built during the historic district's period of significance from 1866-1935 and help demonstrate the growth and development of this portion of the City of Wheeling during that time.

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STATE LEVEL HISTORIC DOCUMENTATION
INDEX TO PHOTOGRAPHS

Cantilevered Sidewalk Segment and Stone Retaining Wall & Culvert Drainage System
US Route 40/National Road
Wheeling
Ohio County, West Virginia

August 2016

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NAT.ROAD - 2	Cantilevered sidewalk with metal railing, looking north
NAT.ROAD - 3	Cantilevered sidewalk over stone wall
NAT.ROAD - 4	Detail of cantilevered sidewalk's metal railing & support beams
NAT.ROAD - 5	Detail of cantilevered sidewalk connection with stone wall
NAT.ROAD - 6	Detail of cantilevered sidewalk supports and of stone wall
NAT.ROAD - 7	Detail of cantilevered sidewalk supports and of stone wall
NAT.ROAD - 8	Stone wall under cantilevered sidewalk, looking northeast
NAT.ROAD - 9	Steel/timber retaining wall, eastern side of roadway
NAT.ROAD - 10	Concrete reinforcement and stairs at northern end of wall
NAT.ROAD - 11	Fill material over southern end of wall and drainage system
NAT.ROAD - 12	Stone arch drainage inlet on eastern side of National Road/US40
NAT.ROAD - 13	Stone arch- concrete pipe connection under National Road/US40
NAT.ROAD - 14	Concrete pipe under National Road/US40
NAT.ROAD - 15	Concrete pipe- corrugated metal pipe connection under cantilevered sidewalk & fill material
NAT.RD.PLAN - 1	WV State Road Commission Plans, showing original location of walls and culvert (1925-1926)
NAT.RD.PLAN - 2	WV State Road Commission Plans, showing original installation of cantilevered sidewalk (1926)



1. Sidewalk along National Road/US40, looking southeast.



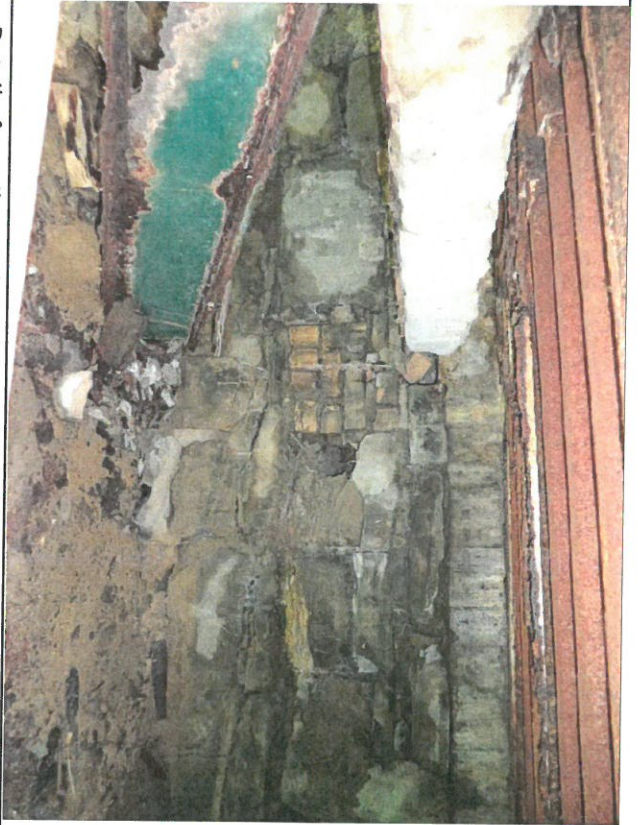
2. Cantilevered sidewalk with metal railing, looking north.



3. Cantilevered sidewalk over stone wall.



4. Detail of cantilevered sidewalk's metal railing & support beams.



5. Detail of cantilevered sidewalk connection with stone wall.



6. Detail of cantilevered sidewalk supports and of stone wall.



7. Detail of cantilevered sidewalk supports and of stone wall.



8. Stone wall under cantilevered sidewalk, looking northeast.



9. Steel/timber retaining wall, eastern side of roadway.



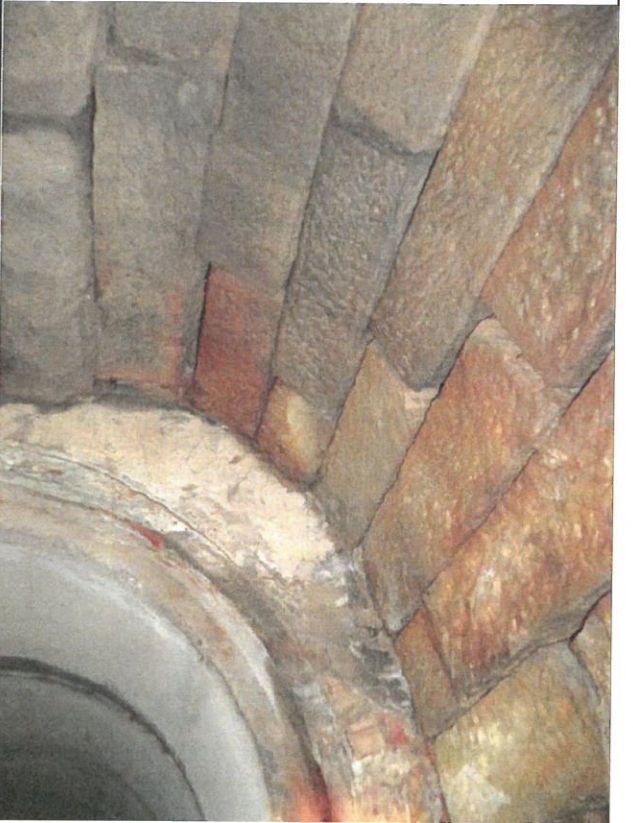
10. Concrete reinforcement and stairs at northern end of wall.



11. Fill material over southern end of wall and drainage system.



12. Stone arch drainage inlet on eastern side of National Road/US40.



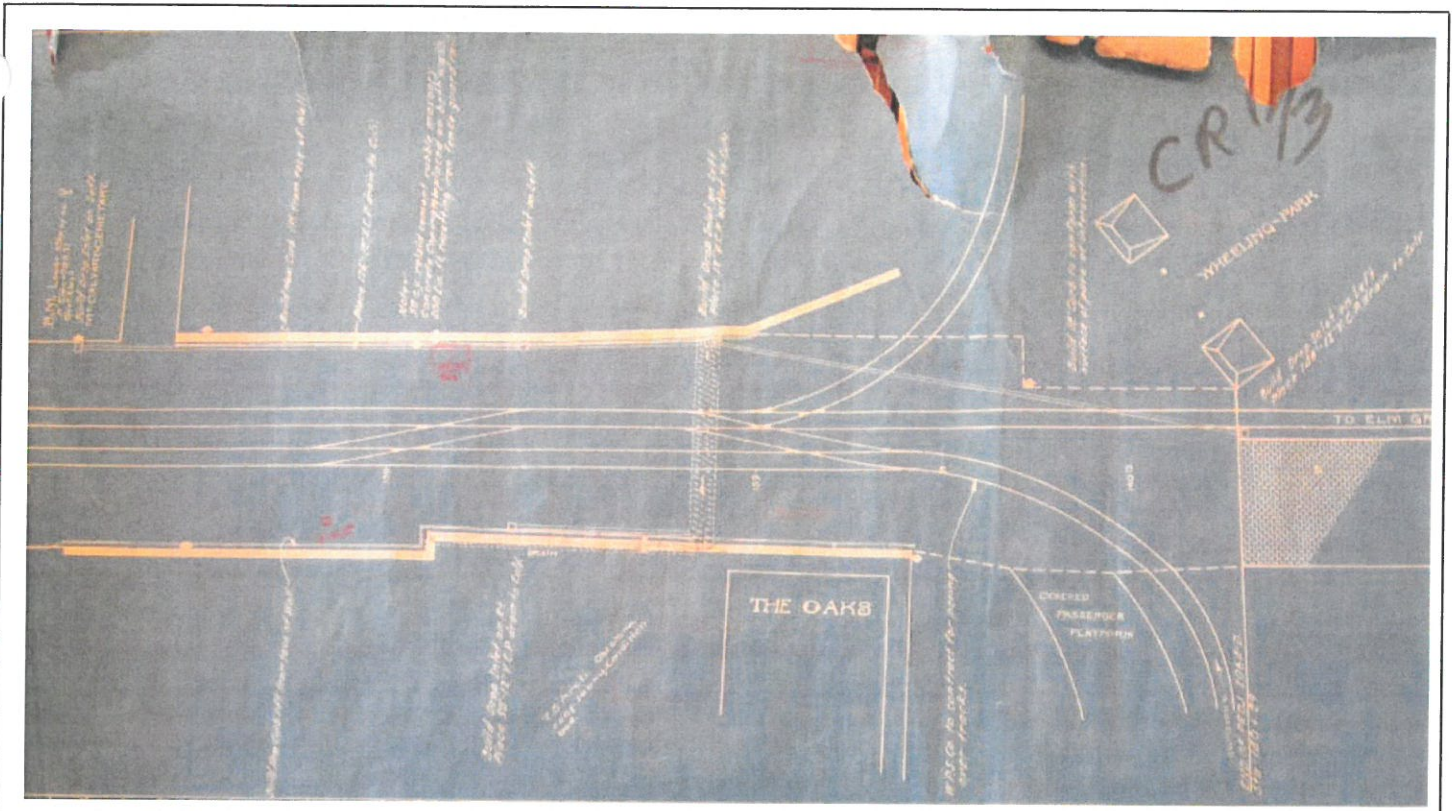
13. Stone arch- concrete pipe connection under National Road/US40.



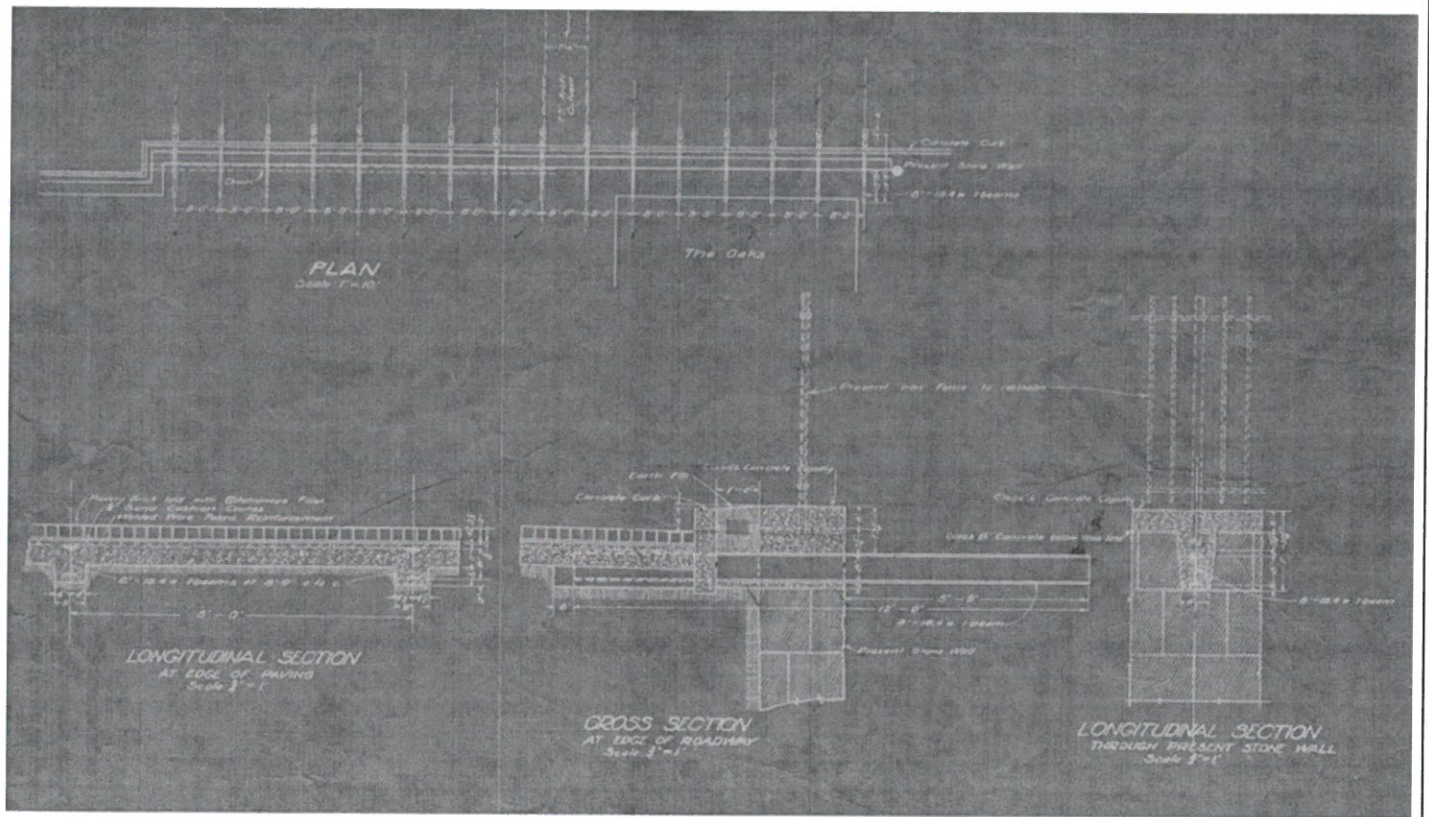
14. Concrete pipe under National Road/US40.



15. Concrete pipe- corrugated metal pipe connection under cantilevered sidewalk & fill material.




1. WV State Road Commission Plans, showing original location of walls and culvert (1925-1926).



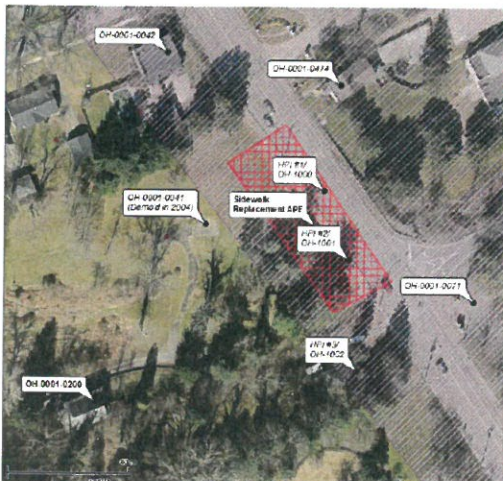
2. WV State Road Commission Plans, showing original installation of cantilevered sidewalk (1926).



WEST VIRGINIA HISTORIC PROPERTY INVENTORY FORM

Street Address National Road/ US40 (Near Mt. Calvary Cemetery & Wheeling Park Entrance)	Common/Historic Name/Both X O O Stone Retaining Wall & Culvert Drainage System	Field Survey # HPI #1	Site # (SHPO Only) OH-1000
Town or Community Wheeling	County Ohio	Negative No.	NR Listed Date
Architect/Builder Unknown/Unknown	Date of Construction c. 1900	Style/Type N/A	
Exterior Siding/Materials Sandstone Blocks	Roofing Material N/A	Foundation N/A	
Property Use or Function Residence O Commercial O Other X <i>Retaining Wall & Drainage</i>	UTM# Zone 17N NAD 1983 527903E, 4434315N		
Survey Organization & Date WVDOH September 2016	Quadrangle Name Wheeling Part of What Survey/FR# National Road/US40 Sidewalk Replacement (FR#: 16-533-OH) State Project # S335-40-3.44 (Mod.)		

Sketch Map of Property
Or Attach Copy of USGS Map



OH-1000

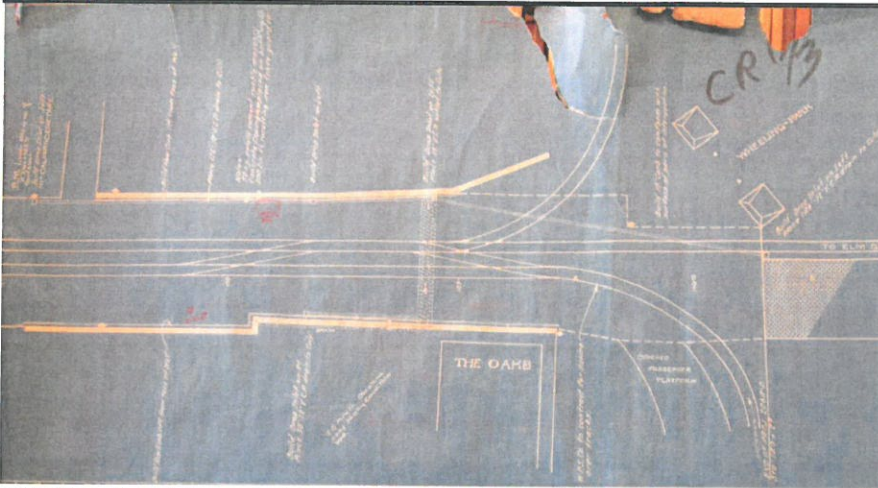
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WEST VIRGINIA HISTORIC PROPERTY FORM CONTINUATION SHEET

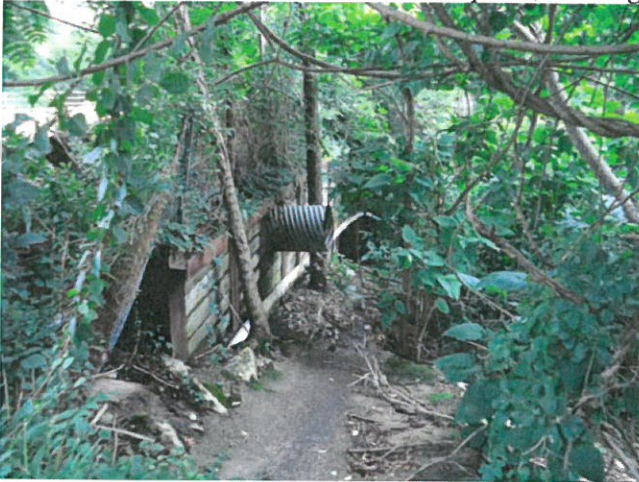
NAME National Road/US40 Sidewalk Replacement

SURVEY NUMBER HPI# 1/OH-1000

PROJECT/ FR# S335-40-3.44 (Modification)/ FR#: 16-533-OH



WV State Road Commission construction plans, showing original location of walls and culvert. (1925-26)



Current steel & timber retaining wall at eastern side of road. (2016)



Concrete pipe that replaced stone arch culvert. (1983)



Concrete reinforcement, drainage, and stairs on northern end of wall. (2016)



Fill material over southern end of wall & metal support beams & concrete coping added to wall. (2016)

WEST VIRGINIA HISTORIC PROPERTY FORM CONTINUATION SHEET

NAME National Road/US40 Sidewalk Replacement

SURVEY NUMBER HPI# 1/OH-1000

PROJECT/ FR# S335-40-3.44 (Modification)/ FR#: 16-533-OH

Statement of Significance

The Stone Retaining Wall and Culvert Drainage System located along the National Road/US40 is not individually eligible for the National Register of Historic Places (NRHP) under any criteria. It is, however, eligible for the NRHP as a contributing structure to the National Road Corridor Historic District, which was listed in 1993.

The National Road was completed from Cumberland, MD to Wheeling, (West) Virginia in 1818. The roadway originally passed through rural farmland with occasional taverns and inns for travelers. The area immediately east of Wheeling began to develop near the turn of the 20th century. Farms were divided up and because of a series of floods in Wheeling, the growth of the local trolley system, and the increase in bicycle and automobile traffic, residential communities began to develop along the National Road. This increase in traffic led to the need for roadway improvement and was likely a factor in the construction of the stone retaining wall and culvert drainage system.

While the structure has some association with the development of Wheeling, its association is not with any specific, significant event. The location of the roadway through this area was previously established and the construction of homes and estates in the area took place prior to this roadway improvement. Therefore, it is not individually eligible under Criterion A for the National Register of Historic Places.

The stone wall and culvert is not known to be associated with the life of any significant historic individual and is therefore not individually eligible under Criterion B for the National Register of Historic Places.

The builder of the stone retaining wall and culvert drainage system is unknown and it does not represent the work of a master or possess high artistic value. The integrity of the wall and culvert system has been diminished over the years as it has been updated and altered and it no longer embodies distinct characteristics of a type, period, or method of construction. Therefore, it is not individually eligible under Criterion C for the National Register of Historic Places.


The stone retaining wall and culvert drainage system is located within the National Road Corridor Historic District which is significant under Criterion C for its residential architecture constructed during a period of significance from 1866-1935. The multiple property listing for the National Road details historic and architectural resources along the National Road. It names significant roadway features as iron mile markers, tapered concrete sign posts, sandstone block walls, and concrete guardrails. This stone retaining wall and culvert drainage system was constructed during the period of significance for the National Road Historic District and helps to demonstrate the growth and development of the area and the City of Wheeling during that time. It is therefore eligible for the NRHP as a contributing structure to the National Road Corridor Historic District.

Bibliographical References

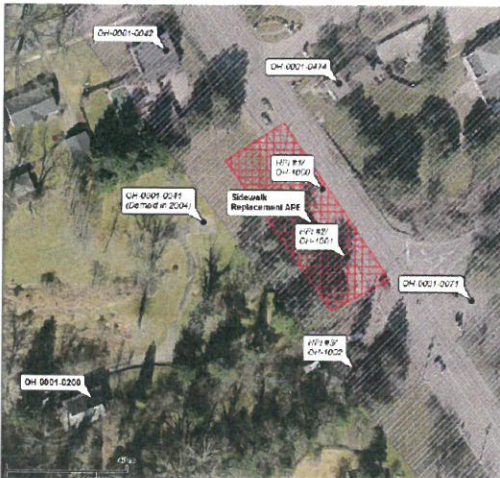
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- WVDOH Engineering Division/ Archive Files



WEST VIRGINIA HISTORIC PROPERTY INVENTORY FORM

Street Address National Road/ US40 (Near Mt. Calvary Cemetery & Wheeling Park Entrance)	Common/Historic Name/Both X O O Cantilevered Sidewalk Segment	Field Survey # HPI #2	Site # (SHPO Only) OH-1001
Town or Community Wheeling	County Ohio	Negative No.	NR Listed Date
Architect/Builder WV State Road Commission	Date of Construction 1926	Style/Type N/A	
Exterior Siding/Materials Steel Beams	Roofing Material Concrete	Foundation Steel Beams	
Property Use or Function Residence O Commercial O Other X <i>Pedestrian Sidewalk</i>	UTM# Zone 17N NAD 1983 527958E, 4434313N		
Survey Organization & Date WVDOH September 2016	Quadrangle Name Wheeling		
Part of What Survey/FR# National Road/US40 Sidewalk Replacement (FR#: 16-533-OH) State Project # S335-40-3.44 (Mod.)			

Sketch Map of Property
Or Attach Copy of USGS Map



OH-1001

Site No.

Present Owners WVDOH Phone #	Owners Mailing Address N/A
Describe Setting N/A Acres Archaeological Artifacts Present The cantilevered sidewalk is located along the National Road/US40 in Wheeling, WV, near Wheeling Park and Mt. Calvary Cemetery.	
Description of Building or Site (Original and Present) N/A Stories N/A Front Bays The cantilevered sidewalk was originally constructed in 1926 along the western side of the National Road. It consisted of 12' long x 8" high steel I-beams, inserted into an existing stone wall and set into concrete below the brick road surface. The 8" I-beams were placed every 8' for a distance of 120' and were cantilevered 5'-9" beyond the face of the existing stone wall. A concrete sidewalk was then laid on top of the I-beams. An iron fence was originally located between the sidewalk and the concrete roadway curb. The iron fence has since been removed and the concrete sidewalk currently abuts the edge of the roadway without a separate curb. Both horizontal and vertical metal beams have been added to the underside of the structure for additional support. The metal railing along the sidewalk is attached to the cantilevered I-beams which extend beyond the outer edge of the concrete sidewalk surface. <div style="text-align: right;"><i>(Use Continuation Sheets)</i></div>	
Alterations	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No If yes, describe The width of the sidewalk has been increased so that there is no division between the sidewalk and the curb of the roadway.
Additions	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No If yes, describe Horizontal and vertical metal beams have been added for additional support under the sidewalk.
Describe All Outbuildings N/A <div style="text-align: right;"><i>(Use Continuation Sheets)</i></div>	
Statement of Significance (See Continuation Sheet) <div style="text-align: right;"><i>(Use Continuation Sheets)</i></div>	
Bibliographical References (See Continuation Sheet) <div style="text-align: right;"><i>(Use Continuation Sheets)</i></div>	
Form Prepared By: Date: September 21, 2016 Name/Organization: Kristin D. Kraus Address: West Virginia Division of Highways 1334 Smith Street Charleston, WV 25301 Phone #: 304-558-9677	



[West Virginia Division of Culture and History](http://www.wvculture.org)
State Historic Preservation Office

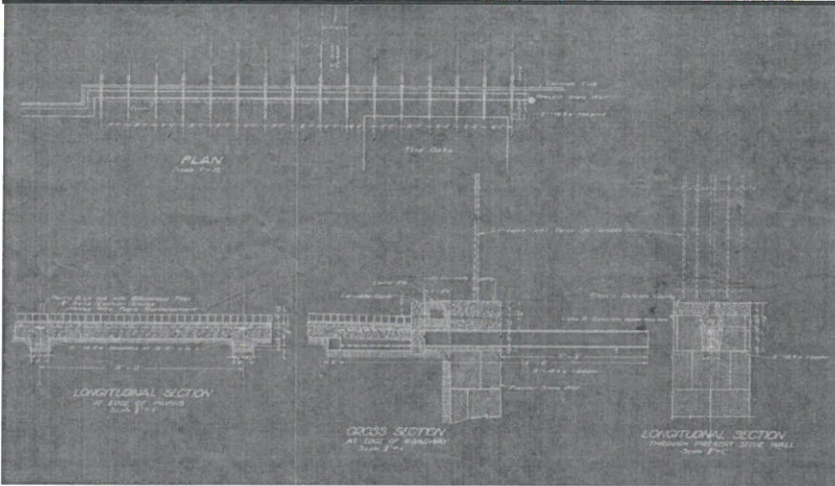
This program receives federal funds from the National Park Service. Regulations of the U. S. Department of the Interior prohibit unlawful discrimination in department Federally Assisted Programs on the basis of race, color, national origin, age or handicap. Any person who believes he or she has been discriminated against in any program, activity, or facility operated by a recipient of Federal Assistance should write to: Director, Equal Opportunity Program, U.S. Department of the Interior, National Park Service, P.O. Box 37127, Washington, D.C. 20013-7127.

WEST VIRGINIA HISTORIC PROPERTY FORM CONTINUATION SHEET

NAME National Road/US40 Sidewalk Replacement

SURVEY NUMBER HPI# 2/OH-1001

PROJECT/ FR# S335-40-3.44 (Modification)/ FR#: 16-533-OH



WV State Road Commission construction plans, showing original installation of cantilevered sidewalk. (1926)



Current sidewalk surface abutting roadway. (2016)



Metal support beams added to underside of sidewalk for additional support. (2016)



Metal support beams added to underside of sidewalk for additional support. (2016)

WEST VIRGINIA HISTORIC PROPERTY FORM CONTINUATION SHEET

NAME National Road/US40 Sidewalk Replacement

SURVEY NUMBER HPI# 2/OH-1001

PROJECT/ FR# S335-40-3.44 (Modification)/ FR#: 16-533-OH

Statement of Significance

The Cantilevered Sidewalk located along the National Road/US40 is not individually eligible for the National Register of Historic Places (NRHP) under any criteria. It is, however, eligible for the NRHP as a contributing structure to the National Road Corridor Historic District, which was listed in 1993.

The National Road was completed from Cumberland, MD to Wheeling, (West) Virginia in 1818. The roadway originally passed through rural farmland with occasional taverns and inns for travelers. The area immediately east of Wheeling began to develop near the turn of the 20th century. Farms were divided up and because of a series of floods in Wheeling, the growth of the local trolley system, and the increase in bicycle and automobile traffic, residential communities began to develop along the National Road. As congress passed a federal aid road act in 1916, numerous roadway improvements were made from 1915-1935.

While the cantilevered sidewalk has some association with the development of the residential communities along the National Road corridor, the association is not with a significant event. The construction of this sidewalk was just one of numerous updates and changes made to the National Road during this time period. Therefore, it is not individually eligible under Criterion A for the National Register of Historic Places.

The sidewalk is not known to be associated with the life of any significant historic individual and is therefore not individually eligible under Criterion B for the National Register of Historic Places.

The sidewalk construction plans were designed by the engineering division of the WV State Road Commission. It does not represent the work of a master or possess high artistic value. The sidewalk has been updated and altered over the years and it does not embody distinct characteristics of a type, period, or method of construction. Therefore, it is not individually eligible under Criterion C for the National Register of Historic Places.

The cantilevered sidewalk is located within the National Road Corridor Historic District which is significant under Criterion C for its residential architecture constructed during a period of significance from 1866-1935. The multiple property listing for the National Road details historic and architectural resources along the National Road. It names significant roadway features as iron mile markers, tapered concrete sign posts, sandstone block walls, and concrete guardrails. While sidewalks along the National Road are not specifically mentioned, the cantilevered sidewalk structure helps demonstrate the growth and development of the area and the City of Wheeling. It was constructed during the period of significance and is eligible for the NRHP as a contributing structure to the National Road Corridor Historic District.

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**MEMORANDUM OF AGREEMENT
BY AND AMONG
THE WEST VIRGINIA STATE HISTORIC PRESERVATION OFFICER
AND THE WEST VIRGINIA DIVISION OF HIGHWAYS
REGARDING IMPLEMENTATION OF THE NATIONAL ROAD/US40 SIDEWALK
REPLACEMENT PROJECT
STATE PROJECT #S335-40-3.44 (Modification)
OHIO COUNTY, WEST VIRGINIA
JANUARY 2017**

WHEREAS, the West Virginia Division of Highways (WVDOH) proposes to replace approximately 183' of sidewalk along the National Road/US40 in Ohio County, hereinafter referred to as the Project. The improvements involve removing the existing sidewalk and its cantilevered support structure, adding fill material over an existing stone wall, and rebuilding the sidewalk on top of the fill material using state funds; and

WHEREAS, the WVDOH has determined that the Project will have an adverse effect upon the Cantilevered Sidewalk Segment and the Stone Retaining Wall & Culvert Drainage System, resources eligible for the National Register of Historic Places (NRHP) as contributing features of the National Road Corridor Historic District; and

WHEREAS, the WVDOH has consulted with the West Virginia State Historic Preservation Officer (WVSHPO) pursuant to West Virginia Code Chapter 29, Article 1 and its implementing regulations (82 CSR 2), as well as 36 CFR Part 800.5 Implementing Section 106 of the National Historic Preservation Act; (16 U.S.C., 470f); and

WHEREAS, the WVDOH has determined that the Project will not affect archaeological properties;

WHEREAS, the WVDOH contacted Friends of Wheeling, the Wheeling Historic Landmarks Commission, the Wheeling Area Historical Society, the Wheeling Area Genealogical Society, the Wheeling National Heritage Area Corporation, the Victorian Wheeling Landmarks Foundation, the Museums of Oglebay Institute, the National Road Alliance of West Virginia, the Belomar Regional Council, the City of Wheeling, and the Preservation Alliance of West Virginia regarding the Project. A response was received from the Friends of Wheeling, stating they have no concerns about the Project.

NOW, THEREFORE, the WVSHPO and the WVDOH agree that the undertaking will be implemented in accordance with the following stipulations in order to take into account the effects of the undertaking on historic properties.

STIPULATIONS

The WVDOH shall ensure that the following stipulations are carried out:

Cantilevered Sidewalk Segment and Stone Retaining Wall & Culvert Drainage System

- I. The Cantilevered Sidewalk Segment and the Stone Retaining Wall & Culvert Drainage System will be documented in their present historic settings. The documentation package will include 5"x7" black and white digital prints in accordance with the National Register of Historic Places and National Historic Landmarks Survey Photo Policy Expansion of March 2005. Also, the documentation will be sent to the Ohio County Public Library in Wheeling, West Virginia.
- II. A brief history of the structures will be included along with fully completed West Virginia Historic Property Inventory forms and copies of plan sheets and drawing of the roadway features from WVDOH files, if they are available.
- III. A brochure describing the contribution of the Cantilevered Sidewalk Segment and of the Stone Retaining Wall & Culvert Drainage System to the National Road Corridor Historic District and the growth and development of the City of Wheeling will be developed by WVDOH and distributed to the Ohio County Public Library in Wheeling and the Friends of Wheeling. The WVSHPO will be given the opportunity to review all educational materials developed for this stipulation.
- IV. The roadway features will be documented on the WVDOH's website, www.highwaysthroughhistory.com.

V. Duration

This MOA will expire if its stipulations are not carried out within five (5) years from the date of its execution. At such time, and prior to work continuing on the undertaking, the WVDOH shall execute a MOA pursuant to 82 CSR 2.5.4.d. Prior to such time, WVDOH may consult with other signatories to reconsider the terms of the MOA and amend it in accordance. WVDOH shall notify the signatories as to the course of action it will pursue.

VI. Post-Review Discoveries

If any unanticipated discoveries of historic properties or archaeological sites, including human burial sites and/or skeletal remains, are encountered during the implementation of this undertaking, work shall be suspended in the area of the discovery until the WVDOH has developed and implemented an appropriate treatment plan in consultation with the WVSHPO pursuant to 36 CFR 800.13 (b).

VII. Monitoring and Reporting

Each year following the execution of this MOA, until it expires or is terminated, WVDOH shall provide all parties to this MOA a summary report detailing work carried out pursuant to its terms. Such report shall include any scheduling changes proposed, any problems encountered, and any disputes and objections received in WVDOH's efforts to carry out the terms of this MOA.

VIII. Amendments

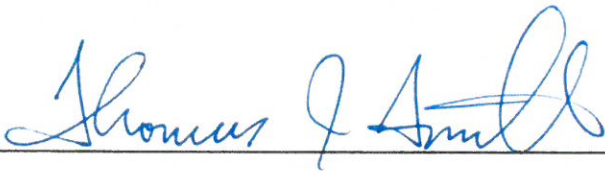
This MOA may be amended when such an amendment is agreed to in writing by all signatories. The amendment will be effective on the date a copy signed by all of the signatories is filed with the SHPO.

IX. Termination

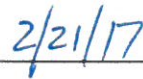
If any signatory to this MOA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other parties to attempt to develop an amendment per Stipulation VIII, above. If within thirty (30) days (or another time period agreed to by all signatories) an amendment cannot be reached, any signatory may terminate the MOA upon written notification to the other signatories.

Once the MOA is terminated, and prior to work continuing on the undertaking, WVDOH must execute a MOA pursuant to 82 CSR 2.5.4.d.

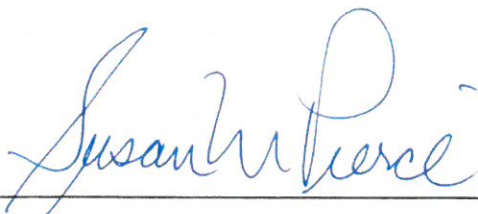
EXECUTION of the Memorandum of Agreement by the WVSHPO and the WVDOH, and implementation of its terms evidence that the WVDOH has afforded the SHPO an opportunity to comment on the National Road/US40 Sidewalk Replacement project and its effects on historic properties, and that the WVDOH has taken into account the effects of the undertaking on the historic properties.



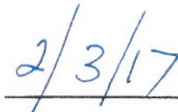
West Virginia Division of Highways



Date



West Virginia Deputy State Historic Preservation Officer



Date

**THE STATE ROAD COMMISSION
OF WEST VIRGINIA**

**PLAN AND PROFILE FOR CONSTRUCTION
OF**

STATE ROAD

U540

PROJECT NO. 2066(2-3)

TRIADDELPHIA DISTRICT OHIO COUNTY

FROM

BLOCH'S STATION TO WHEELING PARK

Sta. 2041+3 to Sta. 2042+7 1/2

Length = 1/2 mi.

SCALE: PLAN 1" = 30' PROFILE 1" = 20' VERTICAL CURVE 1" = 10'

WEST

MP 428 - MP 542

CR 20
MP 542

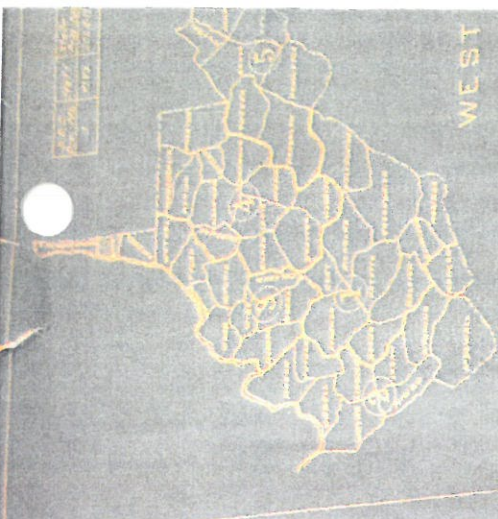
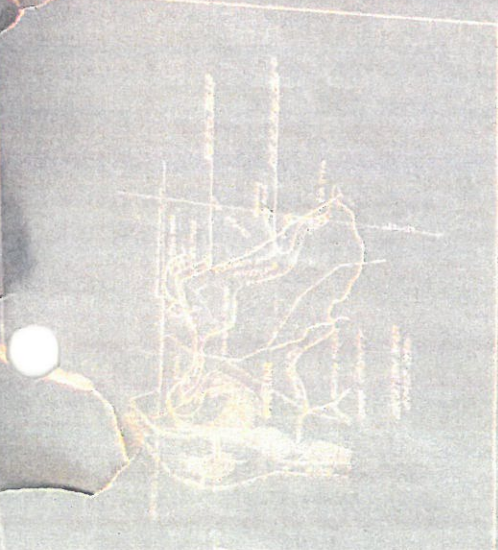
CR 20
MP 428



**LAYOUT
SCALE 1" = 200'**

CONVENTIONAL SIGNS

Symbol	Feature	Symbol	Feature
--- ---	State Line	--- ---	Wall
--- ---	County Line	--- ---	Marsh
--- ---	Corporation Line	--- ---	Hedge
--- ---	District Line	--- ---	Drop Inlet
--- ---	Property Line	--- ---	Bridge
--- ---	Fence Line	--- ---	Percent Culvert
--- ---	Guard Rail	--- ---	Proposed
--- ---	Proposed Road	--- ---	Telegraph Pole
--- ---	Traveled Road	--- ---	Trolley Pole
--- ---	Highway	--- ---	Power Pole
--- ---	Electric Railway	--- ---	Tree



1	2	3	4	5
6	7	8	9	10
11	12	13	14	15
16	17	18	19	20

TYPICAL SECTION OF PAVEMENT
 Scale: 1/4" = 1'-0"



TYPICAL SECTION OF PAVEMENT
 Scale: 1/4" = 1'-0"

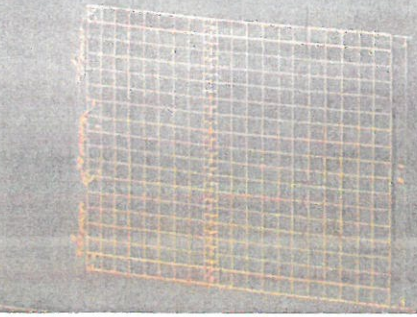


REINFORCED CONCRETE PAVEMENT



ESTIMATE OF QUANTITIES

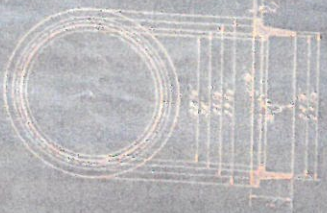
ITEM	QUANTITY	UNIT
Concrete	100	cu yd
Reinforcement	100	lb
Gravel	100	cu yd
Bituminous	100	sq yd



CATCH BASIN
 Scale: 1/4" = 1'-0"



CATCH BASIN
 Scale: 1/4" = 1'-0"



SEWER
 Scale: 1/4" = 1'-0"



SEWER
 Scale: 1/4" = 1'-0"

REINFORCED CONCRETE PAVEMENT



CATCH BASIN FRAME & COVER
 Scale: 1/4" = 1'-0"



CATCH BASIN
 Scale: 1/4" = 1'-0"



CATCH BASIN
 Scale: 1/4" = 1'-0"

CATCH BASIN
 Scale: 1/4" = 1'-0"

THE STATE ROAD COMMISSION OF WEST VIRGINIA.
 STANDARD PLANS AND SPECIFICATIONS TO
 GOVERN CONSTRUCTION THROUGHOUT

